

CITY OF FLORENCE PLANNING COMMISSION

RESOLUTION PC 12 12 FPUD 01

RESOLUTION PC 12 13 SUB 01

RESOLUTION PC 12 14 DR 01

RESOLUTION PC 12 12 FPUD 01: A REQUEST FOR FINAL PLANNED UNIT DEVELOPMENT APPROVALS WHICH INCLUDES A 17-ACRE MIXED USE DEVELOPMENT LOCATED AT 87344 MUNSEL LAKE RD.

RESOLUTION PC 12 13 SUB 01: A REQUEST FOR TENTATIVE SUBDIVISION OF 6.7 ACRES ON THE SOUTHERN END OF THE CANNERY STATION PUD, INTO 7 LOTS.

RESOLUTION PC 12 14 DR 01: A REQUEST FOR DESIGN REVIEW FOR LOT 1 OF THE SEVEN LOT SUBDIVISION (PHASE 1 OF THE FINAL PUD). LOT 1 PROPOSAL INCLUDES A ONE-STORY COMMERCIAL STRUCTURE.

WHEREAS, application was made by Cannery Station LLC and represented by Teresa Bishow of Arlie and Company as required by FCC 10-1, FCC 10-6, FCC 10-23 and FCC 11; and

WHEREAS, the Planning Commission/Design Review Board met in a duly advertised public hearing on May 28 2013 (duly continued until June 25, 2013), to receive evidence and testimony into the record as per FCC 10-1-1-5, and

WHEREAS, the Planning Commission/Design Review Board met in a duly advertised meeting on August 13, 2013 to consider the application, evidence in the record and testimony received as per FCC 10-1-1-5, and

WHEREAS, the Planning Commission/Design Review Board determined per FCC 10-1-1, FCC 10-6-5, FCC 10-7, FCC 10-3, FCC 10-30, FCC 10-23-11, FCC 11-1, FCC 11-3, and FCC 11-4 after review of the application, testimony and evidence in the record, that the application meets the criteria through compliance with certain Conditions of Approval; and

WHEREAS, the Planning Commission/Design Review Board of the City of Florence finds, based on the staff report and evidence and testimony presented to them, that the following conditions are required for full compliance with the applicable criteria:

NOW THEREFORE BE IT RESOLVED that the Planning Commission/Design Review Board of the City of Florence finds, based on the Findings of Fact and the evidence in record that:

APPROVAL INCLUDES:

The application, as presented, meets or can meet applicable City codes and requirements, provided that the following conditions of approval are met.

1. Approval shall be shown on:

Exhibit 1: Findings of Fact

Exhibit 2: Land Use Application Narratives (FPUD and Preliminary Subdivision).
Exhibits 15-36: Land Use Application Exhibits
Exhibit 53: Extent of Wall

Findings of Fact attached as Exhibit "1" are incorporated by reference and adopted in support of this decision. Any modifications to the approved plans or changes of use, except those changes relating to the interior regulated by Building Codes or those needed to comply with conditions of approval, will require approval by the Community Development Director or Planning Commission/Design Review Board. All required site improvements shall be made prior to final inspection and occupancy the City agrees to a performance agreement and financial security.

Final PUD Conditions (2-9):

2. Before installation of any public improvements on any proposed lots east of proposed Redwood Street has begun, the 8-foot wall between Florentine Estates and the proposed development shall be constructed in its entirety as shown in Exhibit 53, unless otherwise conditioned by the Planning Commission.
3. The Final PUD (Phase 1) contains vehicular turn-around areas at the temporary terminus of Spruce and Redwood Streets (See Exhibit 26, Sheet C6.0, Street Plan). These turn-around areas will function for fire and emergency medical and other motor vehicles. As the streets are further extended, the interim turn-around areas must be shifted. If only a segment of 47th Street is constructed, the Final PUD application must include a temporary turn-around area for motor vehicles.
4. Prior to construction of the cantilevered cover for the senior housing facility drop-off, the developer/applicant will need to secure/enter into an agreement with the City of Florence for the private use of the public right-of-way.
5. The applicant shall provide a complete Lighting Plan for public improvements for Phase 1 of the Cannery Station PUD, and a lighting plan for the on-site improvements of Lot 1, and obtain approval of the plans from the Florence Planning Director before building permits are obtained.
6. Unless the Planning Director, Planning Commission or City Council choose to apply current code provisions, the off-street parking provisions of FCC 10-3-2 GENERAL PROVISIONS, 10-3-6 PARKING AREA IMPROVEMENT STANDARD and 10-3-7 OFF-STREET LOADING (as constituted in March, 2008, and included in the findings of fact (Exhibit 1) for PC 12 12 FPUD 01, shall be observed and receive necessary approval for Phase 1 PUD development prior to the issuance of any buildings permits for Cannery Station. This includes the submission of construction plans for parking improvements on Lot 1 which adequately address FCC 10-3-6.
7. To ensure that construction activity for the wall does not encroach upon the existing landscape buffer between Florentine Estates and Cannery Station, prior to any development activity, the developer shall clearly mark, with orange construction fencing, the perimeter of the area of impact.

8. Any future proposed development within the Phase 1 PUD which includes shared parking improvements must adequately address FCC 10-3-5 prior to Design Review approval
9. A timing plan which outlines when improvements for Phase 1 of the Cannery Station PUD are envisioned to be completed must be provided to the Planning Director and approved prior to the issuing of any building permits within Phase I.

Preliminary Subdivision Conditions (10-17):

10. The proposed water line must be sized in accordance with the Oregon Fire Code and City of Florence and Siuslaw Valley Fire and Rescue adopted amendments. Minimum fire flow will be determined at the time of building permit submittal.
11. Prior to Final Subdivision Plat Approval for any development abutting 47th Street (unless ODOT determines it is needed sooner), applicant shall construct a new public street connection to Highway 101 (47th Street) approximately 992 feet south of Munsel Lake Road. Before street improvements begin, the applicant shall submit a revised Sheet C6.0 that illustrates the ODOT improvements required based on the new road approach permit required as per ODOT referral comments dated June 24, 2013 (Exhibit 48).
12. Prior to Final Subdivision Plat approval of those areas affected by a wetland, the applicant shall 1) have acquired all required permits, such as removal-fill permit and/or drainage into wetlands from the Department of State Lands (DSL), and from US Army Corps of Engineers (USACOE), and 2) record a deed restriction to prevent current and future impacts to wetland areas with Lane County Deeds and Records and provide a copy to the City.
13. The preliminary subdivision plan does not include a 35-foot setback line from the eastern property line. The setback must be shown on the final subdivision plat to the extent permissible by Oregon Revised Statute 92.
14. The reserve strip at the south end of Spruce Street (adjacent to the southern property line) will be widened to span the entire distance of the 50-foot right-of-way.
15. The applicant will be expected to proceed with final survey and to make preparations for final subdivision approval within the timeframes outlined in FCC 11-3-6, which will necessitate that the applicant submit for Final subdivision plat within 2 years of the date of preliminary plan approval, (May 28, 2015) unless otherwise provided for through approved and allowed extensions from the Planning Commission.
16. In the event that the applicant requests flexibility with regard to site developments including (but not limited to) the construction of street improvements (sidewalks, street trees, landscaping), the City may require the developer/applicant to provide

bonding, a letter of credit, or other performance guarantee to ensure completion of required public improvements in a sum approved by the City as sufficient to cover the costs of the improvements and repairs, including related engineering and incidental expenses, and to cover the cost of City inspection.

17. Before approval of the Phase 1 Final Subdivision Plat, the applicant will submit verification that ODOT has issued the approach permit necessary for 47th Street access at Highway 101.

Design Review Conditions (18):

18. Prior to initiating site development, the applicant shall contact the Confederated Tribes of Coos, Lower Umpqua, and Siuslaw Indians at a least 72 hours' notice prior to any ground disturbing activities, so a tribal representative can be present during these activities; and if any known or suspected cultural resources are encountered during any phase of the project, contact the Tribes immediately.

APPROVED BY THE FLORENCE PLANNING COMMISSION/DESIGN REVIEW BOARD the 13th day of August, 2013.



CHERYL HOILE, Chairperson
Florence Planning Commission



DATE

**STAFF REPORT & FINDINGS
FLORENCE COMMUNITY DEVELOPMENT DEPARTMENT
Planning Commission
Exhibit "1"**

Public Hearing Date: August 13, 2013 **Planner:** Jacob Callister
Date of Report: August 2, 2013

Application: **PC 12 12 FPUD 01 Cannery Station Final Planned Unit
Development (PUD), PC 12 13 SUB 01 Preliminary Subdivision
and PC 12 14 DR 01 Design Review.**

I. PROPOSAL DESCRIPTION

Proposal: The applicant is requesting final development plan approval for Phase I of a planned unit development as well as design review and a preliminary plan approval for a subdivision of 6.7 acres of the overall proposed 17-acre mixed use development. This Phase 1 Final PUD is for the southern portion of the site.

Applicant: Cannery Station LLC, as represented by Teresa Bishow of Arlie & Company (a member of Cannery Station LLC).

Property Owner: Cannery Station, LLC.

Location: 87344 Munsel Lake Road
Map 18-12-14-20, Tax Lot 700
Southeast corner of Munsel Lake Road and Highway 101

Area: 6.7 Acres (of the overall 17 acre PUD proposal)

Comp Plan Designation: North Commercial Node (NCN)

Zoning District: North Commercial District

Surrounding Land Use/Zoning:

Site: Vacant / North Commercial (NC)
North: Vacant / NC
South: Church / NC
East: Florentine Estates / Single-Family District
West: Fred Meyer, Commercial / NC

Florentine Estates, a retirement community, is located immediately to the east of the subject property. A church is located to the south of Cannery Station. Fred Meyer and other commercial uses are located across from the property on the west side of Highway 101. The property to the north is currently vacant.

Exhibit 33, Sheet LA-1 provides context for the area surrounding the property. It shows property lines, existing streets, and natural features. Another exhibit shows major utilities (Exhibit 24, Sheet C3.0). The surrounding topography shows no significant grade changes. The detailed topography on the Existing Conditions Plan (Exhibit 21, Sheet C1.0), shows a 14 to 20 foot ridge that runs from the southeast to the northwest and can be seen from Highway 101. There are no known future streets planned other than those shown within the Plan and the changes proposed for Highway 101. Please refer to Exhibits 5 through 36 for further Final PUD plan detail.

II. NARRATIVE

The applicant is proposing final approval of Phase 1 of the Cannery Station PUD, a 17-acre mixed use development located at the southeast corner of Munsel Lake Road and Highway 101 in north Florence. The mix of uses is intended to increase employment opportunities, provide housing choices, and include retail and services to meet the daily needs of residents in north Florence. Phase 1 is on the south end of the site and includes Lots 1-6, Tracts A, B and C, portions of Redwood Street, Spruce Street and all of 47th Street.

The proposal is accompanied by an application for design review approval for Lot 1 of the overall 32-lots proposed. Also included is a request for Preliminary Subdivision of proposed Lots 1-7, with Lot 7 constituting the remaining portion of the site (to be developed in future phases of the PUD). The project will be completed in phases to allow for the logical extension of public services. Final PUD approval is required based on the preliminary PUD approval provided with Resolution PC 08 09 PUD 01 (November, 2008), and which included 31 conditions and additional requirements for approval. According to Resolution PC 08 09 PUD 01, Condition of Approval #5 states that "Within twelve (12) months following this approval of the Preliminary PUD, the applicant shall file with the Planning Commission a Final PUD and Design Review application for Phase 1."

The Planning Commission's action approving the Preliminary PUD went into effect on November 19, 2008. The decision was to be in effect for one year or until November 19, 2009. Due to difficult economic conditions, on November 16, 2009 the Florence City Council conducted a public hearing on a proposed ordinance granting an automatic two year extension to all approved preliminary subdivisions and PUD's. Following the public hearing, the Council adopted Ordinance No. 19, Series 2009 approving the extensions and declaring an emergency. Cannery Station preliminary PUD was granted an extension to November 19, 2011. The Florence Planning Commission approved a request for a one-time six month extension to May 19, 2012. This Phase 1 Final PUD was submitted within the mandated time frame.

As part of the Cannery Station preliminary PUD approval, the City approved a Phasing Plan allowing for approval of the Preliminary PUD for the property in its entirety and processing of Final PUD with each subdivision in phases. Final PUD, Subdivision, and Design Review applications can be submitted over a ten year period from the date of preliminary PUD approval. As indicated on the Phasing Plan, the order of phases "does not denote or infer the order or actual timing of the development of the PUD...At each phase, further review will occur concerning the logical extension of urban services..." The intent of the Phasing Plan was to provide the framework for designing and constructing improvements in a sequential manner and avoid unnecessary site changes in advance of actual development. The Phasing Plan recognizes that in some cases, "certain utilities may need to be extended through part of the undeveloped portion of the site to serve the phase under construction."

In accordance with Resolution PC 08 09 PUD 01 adopting the preliminary PUD, the Phase 1 Final PUD must demonstrate compliance with applicable regulations and requirements of the Florence City Code in effect as of March 28, 2008 (unless modified during the preliminary PUD).

As noted, Design Review is required at this stage (and each successive stage of the phased subdivision plan).

This Final Phase I PUD includes the following proposed improvements:

- Lot 1 - A one story 4,325 to 5,000 square foot commercial building and associated parking and landscape areas.
- Lot 2 - An assisted living campus consisting of a three story building and eight cottages providing between 75 and 82 housing units. The lot will also contain open space and a parking area.
- Lot 3 - A single attached cottage associated with the assisted living campus.
- Lot 4 - A single attached cottage associated with the assisted living campus.
- Lot 5 - A three story building for 30 to 50 apartments and associate parking and landscape areas. The range in apartments varies to allow for potential ground floor commercial space.
- Lot 6 - A one story 4,000 to 4,600 square foot commercial building and associated parking and landscaping areas.
- Open Space Tract A located in the southwest corner of the development site designed as part of the stormwater management plan and an area for native plants.
- Open Space Tract B located in the southeast corner of the development site designed as part of the stormwater management plan and an area for native plants.
- Open Space Tract C located at the eastern terminus of 47th Street designed as a pocket park.

The Cannery Station, Phase 1 Final PUD also contains improvements to the southern segment of Highway 101, new local streets, and the extension of public utilities.

The project is proposed to include between 42,775 - 55,900 square feet of commercial space. The North Commercial District allows a broad range of uses, and the project includes medical and professional offices, retail uses serving daily needs of residents, restaurants, a branch bank, a possible hotel, and a community center. Residential uses are allowed in the North Commercial District through the PUD process. Residential uses include an assisted living facility / retirement housing (75-92 units), between 70-90 apartments and 20 attached single-family dwellings.

The proposed mixed-use development includes pedestrian-friendly street design, with shared parking opportunities to create efficient parking facilities. Open space amenities are provided and existing wetland areas are preserved to the extent possible. Buildings are attractively designed to create a vibrant community character. Additionally, the residential uses along the eastern portion of the development are intended to provide a compatible transition between the low-density residential uses in Florentine Estates and the commercial uses along Highway 101.

III. KEY POINTS

A. Modifications. One of the express purposes of the PUD process is to enable comprehensive development in which the design of the overall unit permits increased freedom in the placement and uses of buildings and the location of open spaces, circulation facilities, off-street parking areas and other facilities. Modifications that were requested and permitted in the original PUD include the following:

1. **Pavement Width:** The Preliminary PUD (PC 08-09-PUD-01) provided a modification to subdivision street standards (FCC 11-5-1-A), to allow for street design as proposed by the applicant in Exhibits 27, 28 and 29, (Sheets C6.0, C6.1 and C6.2 respectively). These specifically modifications allow:
 - a. Portions of Redwood Street to be less than the minimum standard of 60 feet of right-of-way and 36 feet of pavement, by allowing portions of Redwood Street to be minimum of 57 feet of right-of-way and 32 feet of pavement; and
 - b. Spruce Street to be less than the minimum standard for 34 feet of pavement, by allowing a minimum of 28 feet of pavement.
2. **Lot Size and Dimensions:** The applicant requested and obtained approval from the Planning Commission, through the preliminary PUD, for modifications to the minimum lot size and dimensions to enhance

implementation of Plan policies. The minimum lot size and dimensions as specified in FCC 10-3-5-G & H are modified to allow:

- a. Lot dimensions to be a minimum of 30-feet wide and 55 feet deep for residential Lots 3 & 4 (Lots 23 & 24 in the Preliminary PUD plan).
- b. Lot areas to be a minimum of:
 - 14,500 square feet for commercial Lot 6 (Lot 9 in the Preliminary PUD plan); and
 - 2,500 square feet for residential Lots 3 & 4 (Lots 23 & 24 in Preliminary PUD).

The Preliminary PUD approved modification to the minimum dimensions and lot size for the attached single-family attached homes, are intended to allow home ownership opportunities, preservation of a landscape buffer adjacent to Florentine Estates, and a density suitable for the north commercial node.

- 3. Setbacks:** Along side streets (other than Highway 101 and Munsel Lake Road), the building setback is required to be a minimum of 15 feet with the front 10 feet to be landscaped. The Planning Commission granted a reduction in the building setbacks on all streets within the development, which include Redwood, Spruce and 47th Streets. The modifications will allow portions of the building facades and porches to be less than 15 feet from the right-of-way. (See, Exhibit 26, Sheet C6.0, Street Plan).

These modifications are a key design principle of Cannery Station to provide attractive, walkable public streets. Buildings are oriented towards the street with parking areas to the rear. The setbacks will vary to help provide attractive outdoor areas and interesting articulated building facades or porches.

The minimum setback requirements from side streets as specified in FCC 10-30-5-B were modified to allow:

- Portions of building facades to be up to one foot from the property line/Redwood Street right-of-way with the condition that within the planting and sidewalk area along Redwood Street, an 8-foot sidewalk is provided (as illustrated in Diagram 2 and 3 on Exhibit 43, Modifications --Sidewalk Detail from the Preliminary PUD). A 12.5 foot combined planting and sidewalk area is proposed in Exhibit 28, Sheet C6.1 Typical Sections.
- Portions of the building facades to be up to 8 feet from the property line along the Spruce Street and 47th Street right-of-way with the condition that within the 10.5-foot planting and sidewalk area along Spruce Street and 47th Street, a 6 foot sidewalk is provided (as illustrated in Diagram 2 and 3 on Exhibit 43, Modifications --Sidewalk Detail from the Preliminary PUD). A 10.5 foot combined planting and sidewalk area are proposed in Exhibit 28, Sheet C6.1 Typical Sections.

- Residential porches to be no less than 4 feet from the Spruce Street right-of-way.

4. Maximum Building Height: All buildings in Phase 1 of the PUD will be within the maximum allowed height limit of 38 feet except for requested modifications for a limited amount of architectural features (a 50-foot tower) on buildings on Redwood Street. For more information about the specific modifications being requested and the basis for the required modifications, please refer to information provided above related to Florence Code Section 10-23-5(D)

B. Water Main Size Increase. On June 28, 2013, Public Works Director Mike Miller and Sean Barrett, Siuslaw Valley Fire and Rescue Marshal submitted a joint comment (Exhibit 47), noting the history of water service requirements for the site, and specifically that the proposed 8-inch water line that crosses Hwy 101 will now need to be increased to 12-inch in order to support the necessary fire flows for the development, especially since 'Phase 1' will not connect the water system to Munsel Lake Road. Minimum fire flows for this development are 3,000 – 3,500 gpm. A water main sufficient to meet these flows is required in accordance with **Condition 10**.

In an email dated June 18, the applicant provided supplemental information responding to questions from the first hearing on May 28th, 2013. This included a narrative related to the history of correspondence and agreements between the City and the applicant, and included a construction bid for water improvements which is included as Exhibit 46.

In an email dated July 2, 2013, the applicant forwarded a formal letter from Matt Keenan, of kpff Consulting Engineers which outlines the applicant's concerns and argument for questioning the 12-inch water line requirement. Concerns focus on interpretations of existing agreements and perceived missed opportunities by the City, in the past, to clarify the 12-inch requirement. Mr. Keenan suggests in his letter that the City pay the difference between an 8-inch and 12-inch water line. The applicant also requests that more clarity be provided regarding the specific distance/extent that the 12-inch line will be required. The applicant's e-mail, and Mr. Keenan's letter are attached as Exhibits 49 and 50 respectively.

C. ODOT (Highway 101). On June 24, 2013, staff received written comments from ODOT (Exhibit 48), revising previously submitted comments (May 28, 2013). Following are the key conclusions included in that letter:

- ODOT noted that it would prefer a street design scenario for 47th Street which does not include a median, but rather accommodates two directional access to Highway 101 from the site. ODOT rescinded an earlier suggestion that Redwood Street extend north to Munsel Lake Road. ODOT required that a traffic study be performed to support the preferred two directional access from

47th Street. The applicant's off-site engineer performed the traffic study, as requested by ODOT. The traffic study was conducted on Wednesday, June 12, 2013 and the study results (submitted to ODOT) revealed minimal impacts which support the newly proposed two directional access dynamic at 47th and Highway 101. The traffic study is included as Exhibit 38.

- The revised intersection dynamic for 47th and Highway 101, resulted in the removal of the requirement for a dedicated right turn lane on Highway 101 adjacent to the Cannery Station site. With no street widening requirements identified by ODOT, other improvements, including a vegetated bio-swale, are no longer required by ODOT. Because of the substantial changes to the phasing of the development from the original phasing proposal (starting on the north end of the site near Munsel Lake Road), ODOT has required that the applicant apply for a new road approach permit for 47th and Highway 101. **Condition 11** requires that the applicant submit a revised Sheet C6.0 drawing to the City that illustrates the ODOT improvements required based on the new road approach permit.
- Mike Miller, Florence Public Works Director notes in his referral comments that the Florence Transportation System Plan and Comprehensive Plan require two northbound lanes along the frontage of the site, including a right turn deceleration lane for the entrance into the project from Hwy 101 and at Munsel Lake Road. Currently the two north bound lane section terminates prior to 42nd street and merges into one north bound travel lane. In order to comply with the City Transportation System Plan (TSP), the City will work with ODOT to eventually require the developer to widen the roadway and install curbs/sidewalks to the ultimate alignment in front of the development. There is sufficient right-of-way to accomodate widening of Highway 101 adajcent to the site.

The applicants traffic analysis (Exhibit 38) has provided evidence that suggests (according to ODOT referal comments) that widening is not supported by the proposed traffic impacts for Phase 1. Additionally, the City is working to add the lane widening to the Street System Development Charge (SDC) calculations so that it is a reimbursable expense. Additional impacts from future Phases of Cannery Station are likley to trigger road widening for Highway 101. Sufficient right-of-way exists to allow widening of Highway 101 If the City is unsuccessful in updating the Street SDC list, then the City reserves the right for a non-remonstrance agreement.

D. Wall adjacent to Florentine Estates. Condition 5(i) from the Preliminary PUD required that before installation of public improvements for the "South 47th" or the "Spruce" Phase of the PUD (current Phase 1) has begun, the wall between Florentine Estates and the project site shall be constructed.

This Phase 1 Final PUD includes the design and construction of the wall between Florentine Estates and Cannery Station. Originally the applicant had proposed to limit the immediate construction of the wall to the area within the Phase 1 boundary, with the remainder of the wall proposed to be constructed in a future phase. Feedback from Florentine Estates residents has resulted in a revised proposal and condition (**Condition 2**) for wall construction. The wall is to be constructed in its entirety before any public improvements are begun for any proposed lots east of proposed Redwood Street. The wall will be subject to Design Review approval prior to construction.

- E. Lighting Plan.** The applicant has not submitted a lighting plan for the proposed conceptual development of Phase 1 or for the on-site improvements on Lot 1. The applicant will be required to provide Lighting Plans for both to the Planning Director for approval prior to any building permit issuance. (**Condition 5**).

IV. PUBLIC AND AGENCY NOTIFICATION

Public Notification: Notice was mailed to property owners and residents within 300 feet of the site, and property was posted on May 7th. Notice was published in the Siuslaw News on May 22nd. The public hearing was extended at the May 28th Planning Commission meeting and the record was also kept open. On May 28th, 2013 it was announced that the hearing would be continued on June 25th, 2013. During the June 25th meeting, the public hearing was closed, the record was kept open for seven days, and Planning Commission deliberations and decision was announced to take place on August 13th, 2013.

Public testimony during both hearing dates was focused on issues related to adjacency with Florentine Estates, and was provided predominantly by Florentine Estates residents. Concern was expressed over the timing and nature of the construction of the wall. Additional concern was raised about storm water runoff dynamics and concerns about flooding.

It is noted that the applicant's representative met with a large gathering of Florentine Estate residents on June 12th, 2013. The meeting was an opportunity to present the most current development plans and to respond to resident's concerns and questions. The meeting was well attended (more than 50 residents) and lasted over 90 minutes. The applicant noted that the meeting was a great opportunity for residents to express concerns and that the overall tone was positive. Concerns (and discussion) focused on the wall and drainage, though other topics were addressed including affordable housing, wetlands and access to the west.

In a letter dated July 1, 2013, and attached as Exhibit 51, the Florentine Estates Facilities Operation manager, Jason Nelson formally expressed some concerns about the storm drainage plans for Cannery Station.

Mr. Nelson noted that he is not a technician and did not fully understand the plans, but had concerns about what he was seeing in the plans. The detention/retention ponds at both the North and South ends of the property, under normal circumstances, will carry the water to the city storm drains. He was concerned about the general elevation dynamics of the site's proposed grading plan, which suggested that higher elevations (92 feet on the WSW of the pond and only 88 feet the East side of the North pond) would result in additional flows to the Florentine Estates during prolonged intense rain. He noted that the Florentine Estates pond does not run into a city storm drain, but rather into another pond that dumps directly into Munsel Creek.

Mr. Nelson expressed concern that the proposed grading may result in increased runoff to the Florentine Estates, which already has significant run-off issues.

In response to Mr. Nelson's concerns, staff rely on the evaluation of the plans by the City's Public Works Director, and the applicant's engineer's assurance as a licensed engineer. In an e-mail response submitted into the record on July 2, 2013, the applicant's civil engineer provided a response to Mr. Nelson's concerns. Following is an excerpt from that response which is included in its entirety as Exhibit 52:

"Regarding the first point made in the letter, the top of the SE pond will be graded to 88 feet and include an overflow inlet that will be piped to the city's new 42-inch line at Highway 101. Regarding the second concern in the letter, the top of the NE pond will be graded to 88.34 feet and include an overflow inlet that will be piped to the city's new 30-inch line in Munsel Creek Road.

The proposed drainage system will keep rainwater runoff from flowing east across the property line into Florentine Estates. The issues with flooding at Florentine Estates existing now, and the proposed Cannery Station development, will not contribute to the existing flooding within Florentine Estates."

Agency Notification: On May 7th, referrals were sent to the City of Florence Building Department, Police Department, Code Enforcement and Public Works, and the Siuslaw Valley Fire & Rescue, Central Lincoln PUD, Qwest, Charter Communications, Florence Postal Service, Confederated Tribes, the Lane County Transportation Planning Division, the State of Oregon, Department of State Lands, Transportation, Fish and Wildlife, Environmental Quality and the U.S. Army Corp of Engineers. As of this writing, the following comments have been received:

Siuslaw Valley Fire and Rescue

On May 28, 2013, Siuslaw Valley Fire and Rescue Chief, Sean Barrett, provided a follow up to comments he provided back in May, 2012, with the original submission. He noted the following:

- Buildings that code requires a NFPA 13 system must have them installed. Multi use buildings will most likely require NFPA 13 systems due to the array of possible uses. The senior center will require the system. However single family dwellings do not require NFPA 13 systems. If they are town houses, apartments or other multifamily dwellings they will/may require at least a 13R system depending on construction and code version used.
- The fire district requests the provision of fire hydrants at the entrance of 47th on the south side and one at the apartment entrance on the north side of 47th (in addition to other proposed hydrants).

Florence Public Works Department

On May 13, 2013, Public Works Director Mike Miller submitted the following comments (Exhibit 44), "Since the initial approval in 2008, Public Works has completed a water master plan. Through modeling and noting how owner is wanting to develop the property, the proposed 8-inch water line that crosses Hwy 101 will now need to be 12-inch in order to support the necessary fire flows for the development, especially since 'Phase 1' will not connect the water system to Munsel Lake Road. Minimum fire flows for this development are 3,000 – 3,500 gpm.

Also please note that the drawings submitted are not construction drawings, but will suffice for planning approval. Construction drawings will need to be approved prior to construction. Florence Public Works will need a traffic plan, stormwater management plan, landscaping plans, etc at the time of construction.

Based on Florence Public Work's Preliminary PUD comments in 2008 we offer the remaining follow up:

1. Streets can remain public. We do have concerns in regards to maintenance of the proposed concrete pavement since the City typically does not maintain concrete streets and repairs may be in the form of HMAC (hot mix asphalt cement). However, Public Works is willing to work with the developer and an owners association to ensure the maintenance of the street.
2. In accordance with City Comprehensive Plan, two northbound lanes will be required along the frontage, including a right turn deceleration lane for the entrance into the project from Hwy 101 and at Munsel Lake Road. Currently the two north bound lane section terminates prior to 42nd street and merges into one north bound travel lane. In order to comply with the City Transportation System Plan (TSP), the City will eventually require the developer to widen the roadway and install curbs/sidewalks to the ultimate alignment in front of the development. The City is working to add the lane widening to the Street System Development Charge (SDC) calculations so that it is a reimbursable expense. The developer will not be required to strip the second lane. Striping of the lane(s) will take place at a time and

date in the future when the rest of the improvements (adding the second north bound travel lane from south of 42nd street) are completed. If the City is unsuccessful in updating the Street SDC list, then the City reserves the right for a non-remonstrance agreement.

3. Traffic Signal at Hwy 101 and Munsel Lake Road. Currently the project is listed as an SDC eligible project. If the trip generation from the proposed development causes the warrants to be met for a signal, the developer will be required to construct the improvements, with reimbursement from City street System Development Charges.
4. Construction drawings shall include the following information:
 - Show profiles of all utilities (sanitary sewer, stormwater and water).
 - Show all service lateral locations, including water meters, Fire Department Connection (FDC) and Double Detector Check Valve (DDCV) assembly locations.
 - Field verify sanitary sewer rim and invert elevations on Hwy 101.
 - Show street light design and locations.
 - Show standards for all utilities (sewer, water, stormwater, street lighting).

In a memorandum dated June 28, 2013, Mike Miller, Florence Public Works Director provided a statement, in consultation with the Siuslaw Valley Fire and Rescue Fire Marshal, Sean Barret, related to the issue of water provision. The memorandum is attached as Exhibit 47, and includes the following statement:

“In further conversations with the Fire Marshal, he has stipulated that the minimum fire flow capacity for this site shall be 3,000 gpm. Given that Phase 1 of the development will only have one connection to the City water system, the water main needs to be 12 inches in diameter in order to provide the fire flows.”

The memorandum also provides increased detail regarding the history of correspondence and agreements between the applicant and the City related to water provision.

V. APPLICABLE CODE SECTIONS:

Florence City Code – Zoning Regulations:

FCC, Title 10, Chapter 1, Zoning Administration

FCC, Title 10, Chapter 3, Off-Street Parking (*March, 2008*)

FCC, Title 10, Chapter 7, Special Development Standards (*March, 2008*)

FCC, Title 10, Chapter 23, Planned Unit Development (*March, 2008 and current*)

FCC, Title 10, Chapter 30, North Commercial District (*March, 2008 and current*)

FCC, Title 10, Chapter 6, Design Review

FCC, Title 11, Chapter 1, Subdivision Administration Provisions

FCC, Title 11, Chapter 3, Major Partition, Preliminary Plan Procedure

FCC, Title 11, Chapter 4, Major Partition Map, Plat Procedure
FCC, Title 11, Chapter 5, Platting and Mapping Standards

Other Applicable Criteria:

Compliance with Conditions of Approval for the Preliminary PUD (1-31).
1992 Site Design Policies and Standards (where applicable)
Realization 2020 Florence Comprehensive Plan

VI. REVIEW OF APPLICABLE CRITERIA:

10-23 PLANNED UNIT DEVELOPMENT

10-23-11: APPROVAL OF THE FINAL DEVELOPMENT PLAN:

- 1. Within six (6) months following the approval of the preliminary development plan, the applicant shall file with the Planning Commission a final development plan containing in final form the information required in the preliminary plan. In its discretion and for a good cause, the Planning Commission may extend for six (6) months the period for the filing of the final development plan.**

FINDING: The Planning Commission's action approving the Preliminary PUD went into effect on November 19, 2008. The decision was to be in effect for one year or until November 19, 2009. Due to difficult economic conditions, on November 16, 2009 the Florence City Council conducted a public hearing on a proposed ordinance granting an automatic two year extension to all approved preliminary subdivisions and PUD's. Following the public hearing, the Council adopted Ordinance No. 19, Series 2009 approving the extensions and declaring an emergency. Cannery Station preliminary PUD was granted an extension to November 19, 2011. The Florence Planning Commission approved a request for a one-time six month extension to May 19, 2012. This Phase 1 Final PUD was submitted within the mandated time frame.

- 2. Final development plans shall include plans for proposed:**
 - a. Storm drainage.**
 - b. Sewer and water utilities.**
 - c. Streets, pedestrian ways, trails and paths.**
 - d. Preliminary subdivision plan, if property is proposed to be divided.**
 - e. Open Space and Parklands to be dedicated to the public or held in Homeowner Association ownership. (Ord. No. 2, Series 2011)**

FINDING: As set forth in Section 10-23-11, the Phase 1 Final PUD contains conceptual plans for public improvements that were prepared by a Registered Engineer and submitted for approval by City staff including:

- Storm drainage
- Sewer and water utilities

- Streets and pedestrian ways

A Preliminary subdivision plan is being reviewed concurrently with the Final PUD as well, and an Open Space plan has been submitted with the Final PUD.

- 3. Plans for public improvements shall be prepared by a Registered Engineer and shall be approved by City staff before final approval by the Planning Commission.**

FINDING: City of Florence Public Works has provided referral comments and noted that the provision of adequate public facilities can be provided, as conditioned (including the provision of more detailed construction plans during the construction phase of the project). Plans have been prepared by a registered engineer.

- 4. If the Planning Commission finds evidence of a material deviation from the preliminary development plan, the Planning Commission shall advise the applicant to submit an application for amendment of the planned unit development. An amendment shall be considered in the same manner as an original application.**

FINDING: These Findings of Fact will show that the Phase 1 Final PUD is in substantial conformance with the Preliminary PUD and standards applicable as of March 28, 2008 (as conditioned, and where applicable). There is no material deviation from the preliminary development plan.

Compliance with Preliminary PUD Conditions of Approval (from Resolution PC 08 09 PUD 01):

- 1. Approval shall be shown on:** (*Exhibit order from preliminary PUD is not consistent with Final PUD exhibits.*)

Exhibit 1: Findings of Fact

Exhibit 3: Land Use Application -- Exhibit D *Draft CC&Rs*, Dated March 26, 2008 (to be amended to meet Condition 13)

Exhibit 3: Land Use Application -- Exhibit H *Traffic Impact Analysis (TIA)*, Dated Aug. 8, 2008

Exhibit 30: Revised TIA Figure 1C, *RIRO Access from Hwy. 101*; and Figure 1D, *North Entry (Redwood Street) from Munsel Lake Road* of Applicant FINDING Letter from JRH

Exhibit 44: Applicant FINDING -- Letter from JRH Regarding: *Cannery Hill Off-site Improvements*, Dated Oct. 14, 2008

Exhibit 45: Land Use Application Drawings, Revised and Submitted Oct. 14, 2008 (with amendments to residential lots 29 – 32 from 1,900 s.f. to 2,500 s.f., and to the location of the wall on the eastern property line from the south end of the property to a point parallel to the north end of Lot 451 in Florentine Estates)

Findings of Fact attached as Exhibit “1” are incorporated by reference and adopted in support of this decision. Any modifications to the approved plans or changes of use, except those changes relating to the interior

regulated by Building Codes or those needed to comply with conditions of approval, will require approval by the Community Development Director or Planning Commission/Design Review Board. All required site improvements shall be made prior to final inspection and occupancy unless the City agrees to a performance agreement and financial security.

FINDING: Approvals were shown on all indicated documents and exhibits. Criteria met.

2. **Regardless of the content of material presented for this Planning Commission decision, including application text and exhibits, staff reports, testimony and/or discussions; the Applicant agrees to comply with all regulations and requirements of the Florence City Code which are current on the date the application was submitted (March 28, 2008), EXCEPT where modification or deviation from such regulations and requirements has been specifically approved by formal Planning Commission action as documented by the records of this decision and the associated Conditions of Approval. In the event that modifications to the approved plans or change of use are approved by the Community Development Director or Planning Commission/Design Review Board, the codes in place at the time the application for a modification is submitted will apply.**

FINDING: Condition 2 is met. These Findings of Fact will show that the Phase 1 Final PUD is in substantial conformance with the Preliminary PUD and standards applicable as of March 28, 2008 (as conditioned).

3. **Off-site roadway improvements shall include the following, as illustrated in Exhibit 30: Revised TIA Figure 1C, *RIRO Access from Hwy. 101*, and Figure 1D, *North Entry (Redwood Street) from Munsel Lake Road*, Exhibit 45 Sheet C6.0 *Street Plan*, and Exhibit 49 *Munsel Lake Road Improvements (lane widths shall meet County requirements)*. (*Exhibit order from preliminary PUD is not consistent with Final PUD exhibits.*)**
 - a. **A new public all-way access street connection to Munsel Lake Road (Redwood Street) approximately 400 feet east of Highway 101 and 290 feet west of Spruce Street.**
 - b. **Dedication of right-of-way and roadway widening of Munsel Lake Road in order to provide a three-lane section between Highway 101 and the eastern property line of the subject property to accommodate separate westbound left turn lanes on Munsel Lake Road at Redwood Street and at Highway 101. Road-widening will also include a bicycle lane, landscape planting strip and sidewalk on the south side of Munsel Lake Road. Dedication and roadway width on Munsel Lake Road shall meet county TSP and Lane Code Chapter 15 requirements and provide for a minimum right-of-way width of 70 feet, and a building setback a minimum of 30 feet**

from the right-of-way.

- c. **A new public right-in-right-out street connection to Highway 101 (47th Street) approximately 992 feet south of Munsel Lake Road. Note: This condition does not limit the developer from seeking ODOT approval for a street connection that provides greater access.**
- d. **Roadway widening to Highway 101 in order to provide a second northbound through-lane from the southern end of the property to the Munsel Lake Road intersection; the second northbound through-lane will act as a de-facto right turn lane at 47th and at Munsel Lake Road because there is currently only one north-bound lane on Highway 101 north and south of the project site. A striping and traffic control plan for interim conditions will be necessary until widening occurs north and south of the project site with ODOT coordination.**
- e. **Urban style improvements with bike lanes, sidewalk, planter strips, curbs and gutters that extends the entire length of the property frontage on Highway 101 and on Munsel Lake Road.**

FINDINGS: The applicant completed preliminary drawings for all of the above off-site improvements as reflected on the Final PUD site plan. The new public street connection to Munsel Lake Road (Redwood Street) will be constructed as part of a future phase. The dedication of needed right-of-way on Munsel Lake Road will occur as part of the subdivision process that creates any new lots adjacent to Munsel Lake Road (not the current subdivision being proposed). Improvements to Munsel Lake Road will be done as part of a future phase when development in Cannery Station occurs in the northern portion of the site.

On January 4, 2010, ODOT approved a new public street connection to Highway 101 (47th Street). On January 4, 2010 ODOT approved the Munsel Lake Road approach permit and set forth required mitigation measures.

On September 9, 2010, ODOT extended the deadline for generating construction drawings for the 47th Street road approach permit (ODOT application number 7905). Subsequently, ODOT has granted additional six month extensions. The current deadline is August, 2013.

On September 9, 2010, ODOT issued a letter stating that the approach permit for Munsel Lake Road is no longer required and will no longer be processed under Oregon Administrative Rule (OAR) Chapter 734, Division 51. The letter explained that on March 4, 2010, the governor signed Senate Bill 1024 into law. Senate Bill 1024 made changes to Oregon Revised Statute (ORS) 374.310 and changed ODOT's authority to manage highway access. According to the letter from ODOT, the new law eliminates ODOT's authority to require that a local jurisdiction obtain a Permit to Operate, Maintain, and Use a State Highway Approach for an existing or

planned city street or county road connection to a state highway.

Phase 1 of the Cannery Station PUD was changed from being on the north side of the site to being on the south end of the site. Initially, as part of this Phase 1 Final PUD, the southern portion of Highway 101 abutting the phase boundary was proposed to be widened and improved in compliance with ODOT recommendations. ODOT revised its referral comments to suggest the removal of the median proposed, and to not require the vegetated swale anymore. ODOT has requested that the applicant apply for a new road approach permit for 47th and Highway 101. **Condition 11** requires that the applicant submit a drawing to the City that illustrates the ODOT improvements required based on the new road approach permit required as per ODOT referral comments dated June 24, 2013 (Exhibit 48).

- 4. Maintenance of the Highway 101 right-of-way must be coordinated between the applicant and ODOT as the City will not be responsible for maintaining the Highway 101 right-of-way.**

FINDING: Condition 4 will be addressed during ODOT's review of required permits for Highway 101.

- 5. Final PUD is authorized to be processed in phases along with each subdivision application (per FCC 10-23-10 last paragraph), with the following conditions:**
 - a. Within twelve (12) months following this approval of the Preliminary PUD, the applicant shall file with the Planning Commission a Final PUD and Design Review application for Phase 1. Within three (3) years following the approval of the Final PUD for Phase 1, the applicant shall file with the Planning Commission a Final PUD and Design Review application for Phase 2. Subsequent phases shall continue to be submitted within eighteen (18) months of the prior approved phase. The Final PUD and Design Review application for the last remaining phase shall be submitted within ten (10) years following this approval of the Preliminary PUD.**

FINDING: The Cannery Station preliminary PUD was granted an automatic two year extension to November 19, 2011. The Planning Commission later granted a one-time six month extension to May 19, 2012. This Final PUD and Design Review application was submitted within the required time frame.

- b. Phasing is not bound by the sequence or time-frame anticipated for each phase, as presented in Exhibit 45, Sheet A1.3 of the Preliminary PUD application, with the condition that the Final PUD and Design Review application for the last remaining phase shall be submitted within ten (10) years following the approval of the Preliminary PUD.**

FINDING: This Final PUD includes the southern portion of the site. It includes an entrance from Highway 101 at the new 47th Street. It is a logical place to begin development and will not hinder the remaining phases from being submitted within ten years of the extended expiration of the Preliminary PUD or by May 18, 2022. Phase I is in compliance with this condition.

- c. At each phase of the Final PUD, the applicant shall demonstrate substantial compliance with the approved Preliminary PUD and all applicable conditions.**

FINDING: As demonstrated in this Final PUD, Phase 1 is in substantial conformance with the approved Preliminary PUD and all applicable conditions.

- d. At each phase of the Final PUD, the applicant shall demonstrate that all public services can be provided.**

FINDING: In April 2010, the City of Florence and Cannery Station LLC reached a mutual agreement on the how City services would be provided to the subject property in addition to improvements completed as part of the Spruce Street LID. The preliminary utility plan submitted with this Final PUD reflects provision of an 8-inch water line to the site (Exhibit 24, Sheet C3.0, Utility Plan.) The Applicant will be required to install a water line which meets fire flow requirements as stipulated by the Fire Marshal. **Condition 10** reflects this requirement.

- e. At each phase of the Final PUD, the applicant shall provide monitoring information and traffic data in order to assess needed mitigation of traffic impacts for that development phase, and propose appropriate mitigation for that development phase.**

FINDING: Cannery Station is currently vacant and there is no traffic being generated at the site. This condition is not applicable to this Phase 1 Final PUD. In future phases, traffic data may need to be collected to evaluate impacts for proposed additional developments.

- f. At each phase of the Final PUD, the applicant shall provide a supplemental analysis assuring operational safety and adequate geometric design of intersections shall be provided for on-site circulation and delivery truck as well as any proposed transit vehicle access.**

FINDING: The development included in this phase does not trigger the need for any supplemental analysis.

- g. At each phase of the Final PUD, the applicant shall provide a calculation of the square footage proposed for each of the land uses within the**

current phase, as well as the cumulative calculation of the square footage that has been achieved in any previously approved and/or developed phases. Any increases to the maximum land use intensities described above require approval from the Planning Commission and the applicant to submit new traffic calculations showing, as a result of the proposed changes that projected p.m. peak hour trips from the combination of the proposed uses will be less than or equal to 589 trips, based on the Institute of Transportation Engineer's Trip Generation Manual.

FINDING: The proposed land use intensities are significantly below the maximum allowed land uses for the entire development site as summarized below:

	Commercial Square Footage	Senior Housing Units	Duets/ Townhouses Units	Mid-Rise Apartments
Maximum Intensity	75,900	82	20	90
Final (Phase 1)	11,825 -16,000	75-82	2	40-50
Preliminary (for Lots in Phase 1)	11,825 -16,000	75-82	2	40-50

There is no need to conduct a traffic study at this stage. Please refer to Exhibit 15, Sheet A1.1, Land Use Plan

- h. At each phase of the Final PUD, the applicant shall demonstrate that a minimum of 20 percent of the net developable area within the current phase combined with any previously approved phases will be dedicated as common open space to be owned in accordance with FCC 10-23-5-G and maintained in accordance with FCC 10-23-6.**

FINDING: The above condition of approval was based on the assumption that Phase 1 would be the northern portion of the development site including the Open Space Tract A. This Phase 1 Final PUD contains .97 acres of dedicated open space within the approximately 4.9 total net developable acres. Dedicated open space, therefore, constitutes 20% of the total net developable area for Cannery Station PUD Phase 1. The location and configuration of the open space for this portion of Cannery Station is substantially consistent with the Preliminary PUD. This Phase 1 Final PUD does not hinder or prevent achieving the amount of needed open space.

Additional small open space areas within blocks are identified in the Plan as well. Areas of open space within blocks (non-dedicated) contribute an additional 1.8% of open space to the overall PUD. These areas within Blocks will be developed as part of the design and construction of each Block. Exhibit 16, Sheet A1.2, includes a table that lists the size of each open space area. The small open space areas are identified by the letters "OS" (open space) and the Lot Number. Please refer to

Exhibit 16, Sheet A1.2, Open Space Plan.

The Landscape Plan for Lot 1 (Exhibit 36, Sheet L-1) shows how open space OS-1 is developed with the design for the building. A plaza is created at the intersection of Redwood Street and 47th Street with large planters, a bench, and bike rack.

The preliminary PUD noted that, "in the event that a pedestrian connection is requested by Florentine Estates Home Owners Association, it will not subtract from the 20% of Open Space."

- i. Before installation of public improvements for the "South 47th" or the "Spruce" Phase of the PUD (as illustrated in Exhibit 45, Sheet A1.3) has begun, the wall between Florentine Estates and the project site shall be constructed.**

FINDING: This Phase 1 Final PUD originally included the design and construction of the wall between Florentine Estates and Cannery Station limited to the area within the phase boundary, with the remainder of the wall proposed to be constructed in a future phase. The applicant outlined a number of reasons for pursuing this approach which included the fact that construction of the entire length of wall prematurely would necessitate the premature removal of significant vegetation, including a nice stand of mature trees, and also that construction of the wall would likely not be based on final grading for the entire length of the wall, which could create a challenging drainage dynamic. Additionally, without the area being landscaped, there would be a high likelihood of non-native and noxious weeds to encroach into the area.

Feedback from Florentine Estates residents indicated sentiment that the wall should be built in its entirety before there is any construction east of Redwood Street has resulted in a revised proposal from the applicant and a condition (**Condition 2**) for wall construction. The wall is to be constructed in its entirety before any public improvements are begun for any proposed lots east of proposed Redwood Street. The wall will be subject to Design Review approval prior to construction.

- 6. The pavement on Redwood Street shall be designed to handle the size and weight of any proposed transit vehicle expected to be used with consideration to the annual number of trips to be made by the service.**

FINDING: The Public Works Department notes in their comments that the plans submitted are sufficient for planning approval but are not construction plans. These plans will need to be submitted at the time of construction and will be evaluated at that time.

- 7. A minimum of one covered parking space per single-family home on lots 13 through 32 (per FCC 10-23-5-E) is allowed with the condition that: 1) the**

driveway is a minimum of 19 feet in depth to allow a second car to park on-site, and 2) the required covered parking space shall not be converted into a living space.

FINDING: Condition 7 applies to Lots 3 and 4 of proposed Phase 1 (formerly Lots 23 & 24 from the Preliminary development plan). Each single family home within the proposal has one covered garage space, and a driveway space sufficient to accommodate a second vehicle.

- 8. If the cottage-style units on the assisted living campus (lot 2) include a garage or carport, the following driveway standards, measured from the property line shall apply:**
- **Garage: 19-foot minimum driveway length; and**
 - **Carport: 3-foot maximum or 19-foot minimum driveway length, and where carports are located 3 feet from the property line, the carport shall not have walls or doors forward of the building setback line. (A carport is an open-sided automobile shelter.)**

FINDING: Any garage or carports associated with the cottage-style units on the assisted living campus will be designed to comply with the above condition. Initially, the entrances to the cottages facing Spruce Street may be limited to pedestrian access until both sides of the street are fully improved and the wall is constructed between Florentine Estates and Cannery Station. These dynamics will be evaluated as part of the Design Review for Lots 2, 3 and 4.

- 9. Prior to application for Phase I Final PUD, the applicant will present to the Community Development Department a signed "Acceptance Agreement" of all conditions of approval.**

FINDING: Condition 9 is met. The applicant signed an "Acceptance Agreement" that is on record at the planning office.

- 10. Prior to application for Phase I Final PUD, the applicant shall have applied for all necessary removal-fill permits and/or drainage into wetlands from the Department of State Lands and from the US Army Corps of Engineers.**

FINDING: The Joint Permit Application (JPA) for necessary removal fill permits and/or drainage into wetlands, has been applied for. The JPA has been signed by all required jurisdictions and the owner has paid the application fees. The JPA was submitted to US Army Corps of Engineers, Portland District, and Oregon Department of State Lands. A copy of the complete application is included in the Exhibits (Exhibit 9).

11. Prior to Phase I Final PUD approval, the applicant shall change the project name from Cannery Hill to Munsel Crossing or another more appropriate project name in order to avoid duplication of place names within the City.

FINDING: Condition 11 is met. The project name is Cannery Station. The name of the property owner was also changed to Cannery Station, LLC. The name is not duplicated within the City.

12. Prior to Phase I Final PUD approval, the applicant shall submit plans for the entire 17-acre site to the City and receive approval from Public Works and Community Development for the following:

- a. **An alternate solution to having a sewer pump station at the north end of project and examine the feasibility of providing gravity sewer service to the entire project site;**
- b. **Grading and Erosion Control Plan;**
- c. **Vegetation Clearing and Re-vegetation Plan;**
- d. **Stormwater Management Plan;**
- e. **Utility Plan for Sewer within the public way, which includes field verification of the sanitary sewer rim and invert elevations on Highway 101; and**
- f. **Utility Plan for Water within the public way, which includes an extension of an 8-inch water line southerly along Hwy 101 frontage and terminate at the south property line with standard 2-inch blow-off assembly (if feasible, given site constraints, a fire hydrant is preferred).**

These plans shall include a development schedule to address the phasing of the project.

FINDING: Agreements exist between the City of Florence and Cannery Station LLC related to the provision of City services to the subject property, however, increases in water service requirements to meet fire flow, have raised questions from the applicant about the agreements. With compliance to new **Condition 10**, this Final PUD Phase 1 demonstrates compliance with the above condition. Please refer to Exhibit 24, Sheet C3.0, Utility Plan, and the written agreement (Exhibit 14). The Florence Public Works director has noted in his comments from May 13, 2013 that a number of utility plans submitted for the Final PUD are sufficient for planning purposes, but that more detailed construction plans will need to be submitted at the time of construction.

13. Prior to Phase I Final PUD approval, the CC&Rs will need to be revised to include the following amendments:

- a. **Buildings as defined by FCC 10-1-4 are prohibited within the 35-foot setback from the eastern property boundary next to Florentine Estates;**
- b. **The required covered parking shall not be converting to living space;**

and

- c. **Dedication and maintenance of open space shall be in accordance with FCC 10-23-5-G and FCC 10-23-6.**

FINDING: The draft CC&Rs were revised to incorporate the amendments above and to reflect the new name for the development. Please refer to changes in ARTICLE VII EASEMENTS AND RESTRICTIONS, Sections 7.1 Common Area, 7.2 Maintenance and Repair of Common Areas, 7.4 Building Setback Adjacent to Florentine Estates, and Section 7.5 Covered Parking. (Exhibit 12) Draft Cannery Station CC & R's Condition is met.

- 14. As part of Final PUD and Preliminary Subdivision Plan application for any phase abutting or requiring access to Munsel Lake Road, the applicant shall obtain an access permit to Munsel Lake Road from Lane County and provide for additional right-of-way on Munsel Lake Road to meet a minimum of 70-foot wide right-of-way, and building setback a minimum of 30 feet from the right-of-way line, consistent with Lane Code Chapter 15 as shown on Exhibit 49.**

FINDING: This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable to Phase 1.

- 15. As part of the Final Subdivision Plat application for any phase abutting or requiring access to Munsel Lake Road, the applicant shall dedicate the necessary right-of-way to meet the minimum 70-foot wide right of way for Munsel Lake Road.**

FINDING: This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable for Phase 1.

- 16. Prior to Final Subdivision Plat Approval for any phase abutting or requiring access to Munsel Lake Road, the applicant shall have secured a facility permit for the Munsel Lake Road improvements, and the related construction drawings shall be approved by Lane County.**

FINDING: This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable for Phase 1.

- 17. Prior to Phase I Final Subdivision Plat Approval, applicant shall have secured all approach permits deemed necessary by the Oregon Department of Transportation (ODOT).**

FINDING: Condition 17 is not applicable to the Final PUD but is applicable to the concurrent subdivision process. ODOT has approved approach permits deemed necessary for the full build-out of Cannery Station. For the Phase 1 Final Subdivision Plat, the applicant will submit verification that ODOT has issued the approach permit

necessary for 47th Street access at Highway 101.

18. Prior to issuance of a Certificate of Occupancy for each development phase, applicant shall have mitigated associated traffic impacts as determined and approved by ODOT. The design and construction of the proposed improvements to mitigate the traffic impacts must be approved by ODOT and be consistent with the requirements of the approved permits.

FINDING: Condition 18 is not applicable to the Final PUD or subdivision process and will be reviewed prior to certificate of occupancy, during the building permit process. This condition has been carried over in this approval.

19. Prior to Final Subdivision Plat Approval for any phase abutting 47th Street (unless ODOT determines it is needed sooner), applicant shall construct a new public street connection to Highway 101 (47th Street) approximately 992 feet south of Munsel Lake Road.

FINDING: Condition 19 is not applicable to the Final PUD but is applicable to the concurrent subdivision application. A public street connection to Highway 101 from 47th Street is proposed as part of the Preliminary subdivision plan and will be included in the final subdivision plat (**Condition 11**).

20. Prior to issuance of a Certificate of Occupancy for any phase abutting or requiring access to Munsel Lake Road, all Munsel Lake Road improvements, including pedestrian and landscaping improvements, shall be completed. Applicant shall provide a statement of completion of off-site improvements from Lane County to the City to show that this requirement has been met.

FINDING: Condition 20 is not applicable to this Phase 1 Final PUD since it does not abut or require access to Munsel Lake Road.

21. As part of the Final PUD application for the applicable phase, the south ends of Spruce and Redwood Streets shall be designed to address vehicular turn-around provisions such as a cul-de-sac or other acceptable standard dead-end treatment.

FINDING: The Final PUD (Phase 1) contains vehicular turn-around areas at the temporary terminus of Spruce and Redwood Streets (See Exhibit 26, Sheet C6.0, Street Plan). These turn-around areas will function for fire and emergency medical and other motor vehicles. As the streets are further extended, the interim turn-around areas must be shifted. If only a segment of 47th Street is constructed, the Final PUD application must include a temporary turn-around area for motor vehicles. (**Condition 3**). Condition 21 is met.

22. As part of the Final PUD application for the applicable phase, a pedestrian

refuge shall be incorporated into the design of the median at the intersection of the 47th Street and Highway 101.

FINDING: In referral comments dated June 24, 2013, ODOT noted that it would prefer a street design scenario for 47th Street which does not include a median, but rather accommodates two directional access to Highway 101 from the site. ODOT rescinded an earlier suggestion that Redwood Street extend north to Munsel Lake Road. ODOT required that a traffic study be performed to support the preferred two directional access from 47th Street. The applicant's off-site engineer performed the traffic study, as requested by ODOT. The traffic study was conducted on Wednesday, June 12, 2013 and the study results (submitted to ODOT) revealed minimal impacts which support the newly proposed two directional access dynamic at 47th and Highway 101. The traffic study (Access Analysis) is included as Exhibit 38.

23. Prior to Final Subdivision Plat approval of those areas affected by a wetland, the applicant shall 1) have acquired all required permits, such as removal-fill permit and/or drainage into wetlands from the Department of State Lands (DSL), and from US Army Corps of Engineers (USACOE), and 2) record a deed restriction to prevent current and future impacts to wetland areas with Lane County Deeds and Records and provide a copy to the City.

FINDING: Condition 23 is not applicable to the Final PUD, but is and will be reviewed during the final subdivision process. A joint fill-removal permit has been submitted to DSL as per Condition 10 from the Preliminary PUD (**Condition 12**).

24. Prior to Final PUD approval of each phase, the applicant shall meet the following requirements of the Florence Public Works Department:

- a. Streets shall meet requirements of Public Works in order to be dedicated to the City and for any street proposed to be private, the developer is to provide a means through an owners association, to ensure maintenance of the street and other private infrastructure is provided.**
- b. Construction drawings shall include:**
 - i. Profiles of all utilities (sanitary sewer, stormwater and water);**
 - ii. All service lateral locations, including water meters, Fire Department Connection (FDC) and Double Detector Check Valve (DDCV) assembly locations;**
 - iii. Street light design and locations; and**
 - iv. Standards for all utilities (sewer, water, stormwater, street lighting).**

FINDING: All streets will be dedicated to the City. All streets within Phase 1 are designed according to City street standards and in accordance with the approved Preliminary PUD. Construction drawings will be reviewed during the construction

phase and PEPI process. To contain costs and enable the first building in the project to be economically viable, the developer requests that some of the streets be partially completed with final improvements done as the project is built-out.

Florence Public Works noted in referral comments that the drawings submitted are not construction drawings, but will suffice for planning approval. Construction drawings will need to be approved prior to construction. Florence Public Works will need a traffic plan, stormwater management plan, landscaping plans, etc. at the time of construction. The Florence Public Works Director noted in a follow up email on May 15, 2013, that Florence Public Works does not have any objections to providing the developer flexibility in regards to street trees, sidewalks and landscaping, and that it would be unwise to install brand new landscaping and sidewalks simply to remove/damage/tear them out when it comes time to actually construct the building. The Public Works Director noted that the City should require an improvement bond or other performance guarantee mechanism to ensure that the improvements are completed in the future (**Condition 16**).

25. Prior to Final PUD approval, a zoning variance application shall be submitted by the applicant to the Community Development Department if a variance is requested from the parking aisle width of FCC 10-3 Off-Street Parking and Loading standards in effect on March 28, 2008.

FINDING: The City Code parking standards were updated during the preliminary PUD application. Therefore, Title 10, Chapter 3 Off Street parking and Loading standards in effect on March 28, 2008 (date PUD application was submitted) is used to evaluate the preliminary PUD and was identified as remaining applicable for the final PUD. According to the preliminary PUD findings, the "old" parking code standards apply fully to both Preliminary and Final PUD With this in mind, any deviation from these code standards, such as reduced parking aisle width, was identified as requiring approval through a variance. The parking aisle width requirement in March, 2008 for a 90 degree two way parking area aisle width was 25 feet. The current development code requires a 23-foot parking aisle width (FCC Figure 10-3-9). The applicant has proposed a parking aisle width of 24 feet, while maintaining a 19-foot stall depth. Because the proposal exceeds the current code standards, and because a variance can only be applied to current code, staff recommends that the proposed parking aisle be approved as splitting the difference between the "old" and new code standards.

26. As part of the Spruce Phase Preliminary and Final Subdivision applications, the applicant shall include the 35-foot setback line from the eastern property line on the plan/plat.

FINDING: Condition 26 is not applicable to this Final PUD, but is applicable to the concurrent preliminary subdivision application. The preliminary subdivision plan does not include the 35-foot setback line from the eastern property line. The setback must

be shown on the final subdivision plat insofar as the standards of Oregon Revised Statute 92 will allow. This condition has been carried over in this approval. **(Condition 13)**

27. As part of the Design Review application, a lighting plan shall be submitted to the Community Development Department, which includes pedestrian scale, lower intensity, downward directed lighting, with lighting levels measured in foot-candles.

FINDING: Condition 27 is not applicable to this Final PUD, but is applicable to the concurrent preliminary subdivision and design review applications. The applicant did not submit a lighting plan for the conceptual street improvements of this phase, nor did they submit on-site lighting plans for Lot 1 with their application materials. Staff recommends an additional condition stating that plans for street lights and on-site lighting for Lot 1 must will be reviewed as part of the street construction drawings, at a later date **(Condition 5)**.

28. As part of the Design Review application, building elevations shall be submitted to the Community Development Department and the design and materials will be evaluated for consistency with the coastal village character of Florence.

FINDING: This Final PUD and Design Review application includes building elevations that are compatible with the coastal character of Florence. Colors and exterior building materials proposed for the building on Lot 1 are included as Exhibit 10, (a computer generated color drawing). A material and color sample board will be made available for review for accurate color evaluation. (Please refer to Exhibits 17, 18, 19 and 20, Sheets A2.1, A2.2 A2.3 and A2.4 respectively).

29. Prior to issuance of a building permit, a soils report and verification of soils bearing capacity and a grading plan, including storm-water design is required. The plan must be prepared by a registered engineer.

FINDING: Condition 29 is not applicable to this Final PUD, Preliminary Subdivision and Design Review, and will be reviewed during the building permit process.

30. Prior to initiating site development, the applicant shall submit a permit application to DEQ for a 1200-C construction stormwater permit with Erosion and Sediment Control Plan, and provide a copy of the approved plan and permit to the Community Development Department.

FINDING: An NPDES General Permit 1200-C Application has been submitted to DEQ. Refer to Exhibit 11, NPDES Permit. The approved plan and permit will be required prior to any site development.

31. Prior to initiating site development, the applicant shall contact the

Confederated Tribes of Coos, Lower Umpqua, and Siuslaw Indians at a least 72 hours' notice prior to any ground disturbing activities, so a tribal representative can be present during these activities; and if any known or suspected cultural resources are encountered during any phase of the project, contact the Tribes immediately.

FINDING: Condition 31 is not applicable to this Final PUD since no site development is proposed at this time. This condition has been carried over in this approval **(Condition 18)**.

An evaluation of the Conditions of Approval for the Preliminary PUD, show that the proposed Final PUD is in conformance or can be in conformance with the Preliminary PUD conditions (as further conditioned).

TITLE 11 SUBDIVISION REGULATIONS

11-3-2: PRELIMINARY PLAN REQUIREMENTS:

- A. Application for Preliminary plan approval shall comply with application requirements of FCC 10-1-1-4.**

FINDING: The Preliminary subdivision plan is on 18 x 24 at 1" = 80' scale.

- B. Drafting: The Preliminary plan shall show all pertinent information to scale. The drawing shall be on standard size sheets eighteen inches by twenty four inches (18" x 24"), and at scale of one inch equal to one hundred feet (1" = 100'). The scale may be increased or decreased if necessary, but in all cases the scale shall be standard, being 10, 20, 30, 40, 50 or 60 feet to the inch or multiples of ten (10) of any one of these scales. Preliminary plans for major partitions and subdivisions shall be proposed by a surveyor who is an Oregon registered engineer or Oregon licensed land surveyor. An affidavit of the services of said engineer or land surveyor shall be furnished as part of the Preliminary plan submitted.**

FINDING: The Preliminary subdivision plan has been prepared by an Oregon licensed land surveyor. An affidavit of the services is attached as Exhibit 39. This criterion is met.

- C. Information Required: The application itself or the Preliminary plan must contain the following information with respect to the subject area:**

- 1. Name and block numbering of proposed subdivision. Except for the words, "town", "city", "plat", "court", "addition" or similar words, the name shall be clearly pronounced different than, the name of any other subdivision in the County unless the subject subdivision is contiguous to and platted by the same party that platted the preceding subdivision**

bearing that name. All subdivisions must continue the block numbers of the subdivision of the same name last filed.

FINDING: The name "Cannery Station P.U.D." has been reserved for this subdivision and is shown on the Preliminary plan. There is no other subdivision in the County with the same or a similar name. The Preliminary map indicates that the subdivision is for Phase I of the PUD. The Preliminary subdivision map is included as Exhibit 4. This criterion is met.

- 2. The date, north point and scale of the drawing; a sufficient description to define the location and boundaries of the proposed subdivision or major partition area; and the names of all recorded subdivisions contiguous to such area.**

FINDING: The date, north arrow and scale are shown on the Preliminary plan. The location of the proposed subdivision and the names of the recorded adjoining subdivisions are shown on the Preliminary plan. This criterion is met.

- 3. The names and addresses of the owner and engineer or surveyor.**

FINDING: The names and addresses of the applicant, the owner and the surveyor are shown on the Preliminary plan. This criterion is met.

- 4. The location of existing and proposed right-of-way lines for existing or projected streets as shown on the Master Road Plan.**

FINDING: The location of proposed and existing right of way lines are shown on the Preliminary plan. This criterion is met.

- 5. The locations, names and widths of all existing and proposed streets and roads. Said roads and streets shall be laid out so as to conform to subdivisions and major partitions previously approved for adjoining property as to width, general direction and in other respects unless it is found in the public interest to modify the street or road pattern.**

FINDING: The names and widths of existing and proposed streets are shown on the Preliminary plan. The plan has designed both Spruce Street and Redwood Street to allow for connectivity to the south, if the church property, adjacent to the south, is redeveloped or if the church agrees to connect to the project site. No connection to Florentine Estates (to the east) is proposed, although proposed open space Tract C preserves the potential for a bicycle/pedestrian connection between Florentine Estates and the project site. An 8-foot wall is proposed for separation between the Florentine Estates and the project site (Phase 1).

- 6. Locations and widths of streets and roads held for private use, and all reservations or restrictions relating to such private roads and streets**

FINDING: Proposed streets and widths are shown on the Preliminary plan (Exhibit 4). All proposed streets will be dedicated as public right-of-way. For Phase I, this includes the southern-most sections of Spruce and Redwood Streets and 47th Street. Criterion is met.

- 7. The elevations of all points used to determine contours shall be indicated on the Preliminary plan and said points shall be given to true elevation above mean sea level as determined by the City. The base data used shall be clearly indicated and shall be compatible to City datum, if bench marks are not adjacent. The following intervals are required:**

Contour Intervals Ground Slope

- 1' 0% to 5%**
- 2' 5% to 10%**
- 5' Over 10%**

FINDING: Contours are shown on the Preliminary plan (Exhibit 4). The contours were taken from drawing dated September 10, 2007, Ward Northwest, Inc. The data provided is complete and sufficient. Criterion is met.

- 1. The approximate grades and radii of curves of proposed streets.**

FINDING: The radius of curves of proposed streets are shown on the Preliminary plan. Phase 1 does not include street curves with the exception of the intersection of 47th Street and Highway 101.

- 2. The approximate width and location of all reserve strips and all existing and proposed easements for public utilities.**

FINDING: Location and width of existing and proposed easements are shown on the Preliminary plan (Exhibit 4). The reserve strip at the south end of Spruce Street (adjacent to the southern property line) will be widened to span the entire distance of the 50-foot right-of-way (**Condition 14**).

- 3. The approximate radii of all curves**

FINDING: The radii of all proposed curves are shown on the Preliminary plan (Exhibit 4). Criterion is met.

- 4. The general design of the proposed subdivision or major partition including the approximate dimensions of all proposed lots and parcels.**

FINDING: Approximate dimensions and lot sizes for all lots and open space tracts are shown on the Preliminary plan (Exhibit 4). Criterion is met

- 5. The approximate location of areas subject to inundation or storm water overflow, all areas covered by water, and the location, width and direction of flow of all watercourses.**

FINDING: The location of these areas is included (See attached Exhibits 24, Sheet C 3.0 and Exhibit 33, Sheet LA-1).

- 6. The existing and proposed uses of the property including the location of all existing structures that the applicant intends will remain in the subject area.**

FINDING: Commercial uses will include - Medical/Professional Offices, Retail, Banking, Restaurants. These uses are proposed for Lots 1, 5 and 6. Residential uses will include - Assisted Living, Retirement, Multi-Family and Single Family Housing. These uses are proposed for Lots 2, 3 and 4.

- 7. The domestic water system proposed to be installed including the source, quality and quantity of water if from other than a public water supply.**

FINDING: The proposed water supply will be from the City of Florence. In April 2010, the City of Florence and Cannery Station LLC reached a mutual agreement on the how City services would be provided to the subject property in addition to improvements completed as part of the Spruce Street LID. The preliminary utility plan submitted with the accompanying Final PUD reflects the agreement on the method of providing public services to the site. Please refer to Exhibit 24, Sheet C3.0, Utility Plan. No concerns were raised by Public Works specific to the provision of water.

- 8. All proposals for sewage disposal, flood control and easements or deeds for drainage facility including profiles of proposed drainage ways.**

FINDING: Proposed sewage disposal and drainage is included (See Exhibits 6 and 7). No concerns were raised by Public Works specific to the provision of sewage disposal. Criterion is met.

- 9. All public areas proposed to be dedicated by the applicant and the proposed uses thereof.**

FINDING: Dedicated areas, which include (and are identified as) Open Space Tracts A, B and C are shown on the Preliminary plan (Exhibit 4).

- 10. All public improvements proposed to be made or installed and the time within which such improvements are envisioned to be completed.**

FINDING: All public improvements proposed to be made or installed are shown on submitted plans (including those plans submitted for the Final PUD Phase 1). The applicant provided no form of a timing plan for the proposed improvements. A timing plan which outlines when improvements for Phase 1 of the Cannery Station PUD are envisioned to be completed must be provided to the Planning Director and approved prior to the issuing of any building permits within Phase I. **(Condition 9)**

11. If lot areas are to be graded, a plan showing the nature of cuts and fills and information on the character of the soil.

FINDING: Grading information is submitted. See attached Exhibits 22 and 23, Sheets C2.0 Mass-Grading Plan and C2.1, Grading Plan Lot 1).

12. A legal description and drawing of the boundaries of the entire area owned by the applicant of which the proposed subdivision or major partition is a part, provided that where the proposal comprises all of such area, an affidavit of such fact shall accompany the Preliminary plan.

FINDING: See attached title report for legal description (Exhibit 42) of the subject property. The boundary of the area is shown on the Preliminary plan (Exhibit 4).

11-3-4: APPROVAL OF PRELIMINARY MAJOR PARTITION OR SUBDIVISION:
After giving notice as required by subparagraph 10-1-1-5-B-1 of this Code, the Planning Commission or its designee shall grant approval or deny the major partition Preliminary plan. The hearing decision and further consideration of a similar application shall be governed by paragraphs 10-1-1-5-D and E of this Code. If approval involves implications of new or modified standards or policy, the Planning Commission and not its designee shall render a decision. The Planning Commission may require its designee to submit any Preliminary approval to the Commission for review prior to notification of the applicant. In the event of a denial, the application shall be reviewed by the Planning Commission within forty five (45) days. Approval shall be granted, provided affirmative findings can be made that: (Amd. Ord 30, Series 1990).

- A. The approval does not impede the future best use of the remainder of the property under the same ownership or adversely affect the safe and healthful development of such remainder or any adjoining land or access thereto.**
- B. The Preliminary plan complies with the requirements of this Title, all applicable provisions of the Oregon Revised Statutes, the Florence Zoning Ordinance, the Florence Comprehensive Plan and Policies, as well as the intent and purpose of this Title.**

FINDING: Evaluation of the proposed subdivision within the Preliminarily approved PUD suggests that approval would not impede the future best use of the remainder of the property (Lot 7) under the same ownership or adversely effects its safe and healthful development (and that of lands surrounding). The Preliminary and final PUD processes have addressed many safety and neighborhood health concerns. The Preliminary subdivision plan is also shown by these findings to comply with the requirements of the Florence City Code (as conditioned).

11-3-6: PRELIMINARY PLAN, EFFECTIVE DATE:

Unless appealed, the Planning Commission decisions under this chapter shall become effective on the thirty first day after rendered. The applicant may then proceed with final surveying and preparation for final approval consideration of the major partition map or subdivision plat, as the case may be. Preliminary plan approval shall be effective for two years within which time the application and major partition map or application and subdivision plat must be submitted as required by this Title. An applicant may apply to the Planning Commission for two (2) extensions of twelve (12) months each. A decision to extend the approval shall be based on compliance with the following criteria:

- A. The request for an extension is made in writing prior to expiration of the original approval;**
- B. There are special or unusual circumstances that exist which warrant an extension; and**
- C. No material changes of surrounding land uses or zoning has occurred. Otherwise the entire procedure must be repeated for reconsideration in light of changed conditions that may exist. (Amd. Ord 30, Series 1990). (Amd. Ord. 12, Series 1999).**

FINDING: The applicant will be expected to proceed with final survey and to make preparations for final subdivision approval within the timeframes outlined unless otherwise provided for through approved and allowed extensions from the Planning Commission.

11-5 PLATTING AND MAPPING STANDARDS

11-5-1: STREETS:

- A. All streets shall comply with applicable development standards of Title 10 Chapter 36, Street Standards.**

FINDINGS: As proposed, Spruce Street is designed with 28-foot pavement width and a 50-foot right of way (with parking only on one side of the street). The Florence Subdivision Code (FCC 11-5-1-B) requires “feeder” streets, such as the proposed Spruce Street, to provide a minimum pavement width of 34 feet, and a minimum right-of-way of 50 feet. Additionally, the proposed Redwood Street varies in

pavement and right-of way-width. Along some portions of the street it is less than the Collector Street minimum standard of 60 feet of right-of-way and 36 feet of pavement. The Preliminary PUD (PC 08-09-PUD-01) provided a modification to subdivision street standards (FCC 11-5-1-A), to allow for street design as proposed by the applicant in Exhibits 27, 28 and 29, (Sheets C6.0, C6.1 and C6.2 respectively). These specifically modifications allow:

- a. Portions of Redwood Street to be less than the minimum standard of 60 feet of right-of-way and 36 feet of pavement, by allowing portions of Redwood Street to be minimum of 57 feet of right-of-way and 32 feet of pavement; and
- b. Spruce Street to be less than the minimum standard for 34 feet of pavement, by allowing a minimum of 28 feet of pavement.

B. Slope Easements: Slope easements shall be dedicated in accordance with specifications adopted by the City Council under Section 11-6-1 of this Title.

FINDING: The Grading Plan (Exhibit 22, Sheet C2.0) does not indicate any slope easements.

C. Reserve Strips: The Planning Commission may require the applicant to create a reserve strip controlling the access to a street, said strip to be placed under the jurisdiction of the City Council and the Planning Commission, when the Planning Commission determines that a strip is necessary:

1. To prevent access to abutting land at the end of a street in order to assure the proper extension of the street pattern and the orderly development of land lying beyond the street; or
2. To prevent access to the side of a street on the side where additional width is required to meet the right of way standards provided in the table under subsection B2 above; or
3. To prevent access to land abutting a street of the partition or subdivision, but not within the partition or subdivision itself; or
4. To prevent access to land unsuitable for building development.

FINDING: Location and width of existing and proposed easements are shown on the Preliminary plan. The reserve strip at the south end of Spruce Street (adjacent to the southern property line) will be widened to span the entire distance of the 50-foot right-of-way (**Condition 14**).

11-5-2: LOTS AND PARCELS:

A. Size and Frontage:

1. **General Requirements:** Each lot shall have a minimum width and depth consistent with the lot width and depth standards for the appropriate zoning district.
2. **Area:** Minimum lot size shall be in conformance with the provisions of the Florence Zoning Ordinance. Where either a community water supply or sewer system are not presently provided, the lot area shall be sufficient to meet State and County health standards and the lot area shall be at least twice the number of square feet normally required in the zoning district where the lot is located. Where an oversize lot as described above is required due to lack of services, the Planning Commission may require the developer to submit a plan for later division of said lot(s) into standard six thousand five hundred (6,500) or nine thousand (9,000) square foot lots.
3. **Frontage:** Each lot shall have frontage of not less than fifty feet (50') upon a street, except that a lot on the outer radius of a curved street or facing the circular end of a cul-de-sac shall have frontage of not less than thirty five feet (35') upon a street, measured on the arc. Where either a public water supply or public sewers are not presently provided, the lot frontage shall be sufficient to insure an adequate sized lot to meet State and County requirements

FINDING: Lot Size and Dimensions: The applicant requested and obtained approval from the Planning Commission, through the PUD, for modifications to the minimum lot size and dimensions to enhance implementation of Plan policies. The minimum lot size in the NC District is 20,000 square feet and the minimum lot dimensions are 100 feet in length and depth. The minimum lot size and dimensions as specified in FCC 10-3-5-G & H are modified to allow:

1. Lot dimensions to be a minimum of 30-feet wide and 55 feet deep for residential Lots 3 & 4 (Lots 23 & 24 in the Preliminary PUD plan).
2. Lot areas to be a minimum of:
 - 14,500 square feet for commercial Lot 6 (Lot 9 in the Preliminary PUD plan); and
 - 2,500 square feet for residential Lots 3 & 4 (Lots 23 & 24 in Preliminary PUD).

The Preliminary PUD approved modification to the minimum dimensions and lot size for the single-family attached homes, are intended to allow home ownership opportunities, preservation of a landscape buffer adjacent to Florentine Estates, and a density suitable for the north commercial node. The small residential lots along the eastern edge of the site will help to provide a transition between low-density residential uses in Florentine Estates and the objectives for a vibrant commercial node in Cannery Station. Additionally, in order to allow a diversity of businesses and

employment opportunities and to provide new public streets that foster connectivity, the minimum lot sizes for some of the commercial lots are proposed to be less than the required minimum.

B. Exceptions:

- 1. Subdivisions and Partitions Developed as a Unit: The Planning Commission may in its discretion authorize the relaxation of the lot size and frontage requirements specified herein where the applicant presents a plan satisfactory to the Planning Commission whereby the entire subdivision or partition will be designed and developed with provision for proper maintenance of open space, recreation and parklands and will be commonly available for recreation and park purposes to the residents of the subdivision or partition, and which the Planning Commission determines will be of such benefit to said residents as is equal to that which would be derived from observance of the lot size and frontage requirements otherwise specified, and will be in accordance with the purpose of this Title.**

FINDING: Certain standards are modified (as noted) based on preliminary PUD approval.

11-5-3: PUBLIC FACILITIES:

All utilities shall comply with applicable development standards of Title 10 Chapter 36 and Title 9.

FINDING: City of Florence Public Works has provided referral comments and noted that the provision of adequate public facilities can be provided, as conditioned. The applicant's proposed utility plan notes that "electrical and franchised utilities are to be underground and designed to Central Lincoln People Utility District, Charter Communications, Century Link, etc, standards." Approval will be shown on the utility plan as per **Condition 1**

11-5-4: PARTIAL DEVELOPMENT:

Where the subdivision or partition includes only part of the tract owned by the applicant, the Planning Commission may require a sketch of a Preliminary layout of streets in the remainder of said ownership.

FINDING: The preliminary subdivision application is for Phase 1 of the Cannery Station PUD. The PUD has received preliminary approval and final approval is sought in conjunction with this subdivision application. There is an existing street plan for the remaining portions of the site (Lot 7) (Exhibit 15, Sheet A1.1, Land Use Plan.)

11-5-5: UNSUITABLE AREAS:

Areas identified in the Florence Comprehensive Plan as having designated or protected natural areas or potential hazards due to erosion, landslides, stream flooding, ocean flooding or other natural hazards shall not be divided in a manner that would be dangerous to the health and safety of those who would live in said areas, the general public, or natural values which have been protected.

- A. All major partition and subdivision applications shall be reviewed by the City, using the Phase I checklist contained in Site Investigation Reports by Wilbur E. Ternyik, published by OCZMA.**
- B. Where problem areas are identified in the Phase I checklist, a full-scale Phase II site investigation will be required covering only those problem areas identified in the Phase I checklist. This site investigation must be prepared and paid for by the applicant. Before approval would be granted the site investigation would have to prove either:**
 - 1. That upon specific examination of the site, the condition which was identified in the Comprehensive Plan Inventory did not exist on the subject property; or**
 - 2. That harmful effects could be mitigated or eliminated through, for example, foundation or structure engineering, setbacks or dedication of protected natural areas.**
 - 3. Specifically, areas shown on the Hazards Map and the Soils Map of the Comprehensive Plan will require a Phase II site investigation report. Studies which have been adopted or included in the Comprehensive Plan by reference or studies done subsequent to the adoption of the Plan may be used to determine when a site investigation report is needed.**

FINDING: A site investigation report was conducted during the Preliminary PUD process and the only hazard identified is an earthquake hazard, which covers all of Florence and its environs. To mitigate potential risk from an earthquake, the commercial use buildings will be required to address designs which include seismic provisions of OSSC 1623 or ASCE-7 with appropriate criteria for the site class and location and verification of soils bearing capacity. This findings also shows sufficient compliance with Title 10 Chapter 7, Special Development Standards (March, 2008 code and current).

10- 6 DESIGN REVIEW

10-6-6: DRAWING SUBMITTAL: The owner or authorized agent shall submit the following drawings to the City for review:

- A. A site plan, drawn to scale, showing the proposed layout of structures and other improvements including, where appropriate, driveways, pedestrian walks, off-street parking and off-street loading areas, landscaped areas, locations of entrances and exits,**

the direction of traffic flow into and out of off-street parking space and loading berth, and areas for turning and maneuvering vehicles. The site plan shall indicate how utility services and drainage are to be provided.

- B. A landscape plan, drawn to scale, in conformance with FCC 10-34-3-2.
- C. Architectural drawings or sketches, drawn to scale, including floor plans in sufficient detail to permit computation of yard requirements and showing all elevations of the proposed structures as they will appear upon completion. All exterior surfacing materials and colors shall be specified
- D. Additional information may be required by the City if necessary to determine whether the purposes of this Chapter are being carried out or may authorize omission of any or all the drawings require by this Chapter if they are not necessary. The City shall specify the number of copies of each drawing to be submitted.

FINDING: The required drawings were submitted in conjunction with the Final PUD Phase 1 application. A lighting plan has been requested per **Condition 5**. The building plan and elevation drawings in the PUD submittal illustrate general design and character of the building proposed for Lot 1. Also included are drawings for Lots 2 through 6. These drawings are conceptual and represent examples of design elements for the development within Phase 1. Building designs included in Exhibits 17-20 are representational and not intended to necessarily be the final layout and design. The submission is sufficient for initial review of key design elements of the site. Further design review will be necessary at the time of development of each additional building and additional detail will be required at that time. Development must meet, except where modified through the PUD, the Design Criteria for the North Commercial Node as outlined in FCC 10-30-6.

10-6-1: PURPOSE: The design review process is intended to:

- A. Create an attractive appearance that will enhance the City and promote the general welfare of its citizens.
- B. Provide property owner the means to protect and conserve the architectural tone of their neighborhood.
- C. Recognize areas of existing or potential scenic value.
- D. Protect and preserve buildings and sites that are of significant architectural or historic merit. (OR 625, 6- 30 80)

FINDING: The proposed conceptual design and guidelines for Lot 1 create an attractive appearance. Overall the mixture of sizes and uses conceptually proposed for the Phase will foster a lively center for people to live, work, and visit. Buildings designs, including Lot 1, will integrate architectural elements to seamlessly unite residential and commercial portions of the property.

Buildings will range from one to three stories, and will include significant glazing to enhance visibility of businesses and the attractiveness of the housing units. The ground level commercial spaces are intended to actively participate and contribute to the life of the street, especially Redwood Street. Windows will be clear glass, and varied in size, but will tend toward the large and open. Most windows will have some type of cover, either rigid canopy or awning, or be recessed under the building's volume. Window articulation will relate to traditional rhythms and be inspired by the coastal aesthetic. Lot 1 will initiate many of these themes.

The preliminary PUD allowed for small-scale commercial uses on the ground floor of multi-story buildings on the east side of Redwood Street, including the assisted living facility and the apartment buildings. Due to uncertainties in the market and economic conditions, the applicant requested that the apartment buildings be allowed to be approved without the requirement for non-residential uses on the ground floor. The Planning Commission did not require all of the first floor of the apartment buildings to be commercial. However, it was requested that, at the time of Design Review for those structures, the use for the first floor apartments be identified.

Roof forms will continue to reflect the "cannery" character by way of traditional forms. Roof forms in the residential multi-story buildings are intended to vary in order to contribute toward reducing the block-long massing of the buildings. Colors will tend toward the monochromatic and thereby emphasize the forms and shapes of the buildings themselves. Accent colors against this background may vary, some being bright and striking in selected places. The residential colors shall tend toward the natural tones, but at the commercial levels may be more saturated.

The Florence Comprehensive Plan does not identify any important scenic views from the subject property. The project will not impact any scenic views from adjoining properties due to existing changes in elevation of dense landscaping. The proposed project will not have a negative impact on any important scenic views from motorists on Highway 101.

The Cannery Station site has no existing buildings of historic significance. The applicant must contact the Confederated Tribes of Coos, Lower Umpqua, and Siuslaw Indians at a least 72 hours' notice prior to any ground disturbing activities, so a tribal representative can be present during these activities; and if any known or suspected cultural resources are encountered during any phase of the project, the Tribes must be contacted immediately (**Condition 18**). This criterion is met.

10-6-5: GENERAL CRITERIA: The Board shall consider the following criteria reviewing applications and may set conditions or standards which regulate and limit the following: (Ord. 680, 1-11-83)

- A. Setbacks, yards, height, density and similar design features according to the underlying zoning district.**

FINDING: A one story 4,325 to 5,000 square foot commercial building and associated parking and landscape areas.

Proposed buildings for Lots 2-6 (which will be subject Design Review when more detailed development is proposed) include the following:

- **Lot 2** - An assisted living campus consisting of a three story building and eight cottages providing between 75 and 82 housing units. The lot will also contain open space and a parking area.
- **Lot 3** - A single attached cottage associated with the assisted living campus.
- **Lot 4** - A single attached cottage associated with the assisted living campus.
- **Lot 5** - A three story building for 30 to 50 apartments and associate parking and landscape areas. The range in apartments varies to allow for potential ground floor commercial space
- **Lot 6** - A one story 4,000 to 4,600 square foot commercial building and associated parking and landscaping areas.

Along side streets (other than Highway 101 and Munsel Lake Road), the building setback is required to be a minimum of 15 feet with the front 10 feet to be landscaped. The applicant requested and obtained a reduction in the building setbacks on all streets within the development, which include Redwood, Spruce and 47th Streets. The modifications will allow portions of the building facades and porches to be less than 15 feet from the right-of-way. (See, Exhibit 26, Sheet C6.0, Street Plan).

These modifications are a key design principle of Cannery Station to provide a compact neighborhood with a mix of uses providing life's daily necessities and sufficient density to support transit. The character of the neighborhood is reinforced by enhancing the public realm and providing attractive, walkable public streets. Buildings are oriented towards the street with parking areas to the rear. In general, the project design requires that standard building setbacks along local streets be modified to allow buildings to be close to the street. The setbacks will vary to help provide attractive outdoor areas and interesting articulated building facades or porches.

The minimum setback requirements from side streets as specified in FCC 10-30-5-B were modified to allow:

- Portions of building facades to be up to one foot from the property line/Redwood Street right of way with the condition that within the planting and sidewalk area along Redwood Street, an 8-foot sidewalk is provided (as illustrated in Diagram 2 and 3 on Exhibit 43, Modifications --Sidewalk Detail from the Preliminary PUD). A 12.5 foot combined planting and sidewalk area is proposed in Exhibit 28, Sheet C6.1 Typical Sections.
- Portions of the building facades to be up to 8 feet of the property line along the Spruce Street and 47th Street right-of-way with the condition that within the 10.5-foot planting and sidewalk area along Spruce Street and 47th Street, a 6 foot sidewalk is provided (as illustrated in Diagram 2 and 3 on Exhibit 43, Modifications --Sidewalk Detail from the Preliminary PUD). A 10.5 foot

combined planting and sidewalk area are proposed in Exhibit 28, Sheet C6.1 Typical Sections.

- Residential porches to be no less than 4 feet from the Spruce Street right-of-way.

Maximum Building Height: The design guidelines and conceptual development proposed by the applicant within the maximum allowed height limit in the zoning district, except as noted in the list of requested modifications below.

All structures immediately abutting property zoned residentially (Florentine Estates) are within the allowed 28-foot height limit. The proposed duets on Lots 3 & 4 will not exceed 26 feet in height. The 6 cottages proposed on Lot 2 as part of the assisted living / senior housing facility also abut Florentine Estates and will not exceed 26 feet in height.

All remaining buildings in Phase 1 of the PUD will be within the maximum allowed height limit of 38 feet except for the requested modifications for a limited amount of architectural features (a 50-foot tower) on buildings on Redwood Street. For more information about the specific modifications being requested and the basis for the required modifications, please refer to information provided above related to Florence Code Section 10-23-5(D)

B. Lot area, dimensions and percentage of coverage according to the underlying zoning district.

FINDING:

Lot Area and Dimensions: As noted under the findings for subdivision criteria 11-5-2 LOTS AND PARCELS, the applicant requested and obtained approval from the Planning Commission, through the preliminary PUD, for modifications to the minimum lot size and dimensions. The lot area provisions for the proposal are consistent with the modifications granted.

Building Coverage: In a residential PUD, not more than fifty percent (50%) of the land area being developed, exclusive of public or private streets, shall be covered by buildings. When the PUD is not entirely residential, maximum building coverage shall be consistent with the purpose and general criteria of this Chapter as determined by the Planning Commission.

Cannery Station is a mixed use PUD with residential uses planned primarily along the east side of the site to ensure compatibility with the adjacent existing neighborhood (Florentine Estates). The applicant states that based on the approximate building footprints, about 35 percent of the site planned for residential uses will be covered by buildings. (Percentage based on Lot 2 and Lots 3, 4 and 14-24) planned for residential development. The remainder of the site is intended for non-residential uses or preserved for open space.) Phase 1 is in conformance

with the overall PUD providing no threat to the estimated and approved overall building coverage percentage.

C. Installation and maintenance of fences, walls, hedges, screens and landscaping according to standards set forth in FCC 10-34 Landscaping, and any requirements of the underlying zoning district.

FINDING: Review of the Landscape Master Plan (LA-2) and the Landscape Plan for Lot 1 (L-1) (Exhibits 34 & 36 respectively) shows substantial compliance with the standards outlined in FCC 10-34 Landscaping. It provides a clear indication of the location and size of proposed plants as well as a detailed plant schedule identifying appropriate plant types (including botanical as well as common plant names).

The area along Spruce Street is designed for small scale residential buildings to provide a smooth transition with Florentine Estates. In addition, the application includes an eight (8) foot high solid wall (per city code) between proposed new buildings and Florentine Estates. As part of Phase 1 the proposed wall is planned only for the portion of the site within the Phase 1 boundary. The findings for Preliminary PUD Condition 5 outline the applicants reasoning for this decision. The Planning Commission may decide that the reasoning is not adequate and require the entire wall to be constructed.

Hedges are incorporated as per the *1992 Site Design Policies and Standards*, and provide adequate screening of the site. The proposed landscaping is consistent with the Preliminary development plan approval as well as the *1992 Site Design Policies and Standards*.

D. The location and design of access and egress points for vehicles and pedestrians, including access points along State highways according to standards set forth in FCC 10-35 Access and Circulation, and any requirements of the underlying zoning district.

FINDING: Prior to construction of the cantilevered cover for the senior housing facility drop-off, the developer/applicant will need to secure/enter into an agreement with the City of Florence for the private use of the public right-of-way. **(Condition 4)**

E. Noise, vibration, smoke, dust, odor, light intensity and electrical interference's.

FINDING: Measures are taken to mitigate noise, vibration, smoke, dust, odor, light intensity and electrical interference. The area along the east side of Redwood Street is setback from the highway a sufficient distance to mitigate noise impacts and provide a visual buffer for residents.

F. Parking and outside display areas, dimensions, surfacing and on-site traffic circulation according to standards set forth in FCC 10-3 Parking and Loading.

FINDING: Off-street parking standards are addressed within this staff report under 10-3 OFF STREET PARKING AND LOADING beginning on Page 44. Adequate parking is not a current Design Review issue for Lot 1, but has potential to become an issue as build-out occurs and shared parking dynamics are pursued. .

G. Architectural quality and aesthetic appearance, including compatibility with adjacent buildings.

FINDING: Conceptual residential building character includes buildings along Spruce Street which are smaller in scale and incorporate appropriate coastal elements. Varied roof forms will emphasize individual units and front porches will foster community connections. Commercial building design is inspired by coastal cannery architecture, particularly materials and gabled and shed roof forms. Blank areas of wall may be used as locations for mural signage. Setbacks, bay widths, balconies and roof forms are included and provide variety and break up massing and scale. Towers and taller roof forms mark special corners or parts of buildings.

The design guidelines include large storefront windows engage life on the street. Rigid canopy, awnings or sun shades provide cover and articulate facades. Residential and office windows create traditional window patterns (not horizontal bands). Sizes and rhythms vary to provide visual interest.

H. Color, building materials and exterior appearance in accordance with the policies established by the City in the Downtown Implementation Plan, and in applicable zoning districts.

FINDING: The applicant has provided the following description of colors for the conceptually proposed structures: Commercial buildings are proposed to tend toward the monochromatic to emphasize the building form and shape. Accent colors may vary, some being saturated in select places. Residential buildings shall tend toward natural tones with detail features of more saturated colors. Examples of siding materials include board and batten, cedar shingles, metal siding, stucco, and concrete accents. Examples of roofing materials include metal, composition shingle, and single-ply on low slope roofs not visible from street. The applicant has provided a color example of the proposed building for Lot 1 in Exhibit 10.

I. Exterior lighting and security.

FINDING: The applicant will be required to provide a Lighting Plan for on-site improvements on Lot 1 to the Planning Director. Review and approval of the lighting plan will be required prior the issuing of any building permit on the site (**Condition 5**).

J. Public health, safety and general welfare.

FINDING: Buildings, including any commercial structure, apartment complex and the assisted living center, on the Cannery Station site will be required to have an NFPA

13 sprinkler system installed as per Siuslaw Fire and Rescue Marshall, Sean Barrett. Additionally, the district requires a fire hydrant within 50 feet of the FDC. For proper coverage, with approval from the City of Florence Public Works Director, the fire district requests the provision of fire hydrants at the entrance of 47th Street on the south side and one at the apartment entrance on the north side of 47th Street (in addition to other proposed hydrants). The hydrants and sprinkler system installations are proposed by applicant as noted in the applicant's Compliance Statement (Exhibit 3) and the Exhibit 24, Sheet C3.0, Utility Plan.

K. Provision of public facilities and infrastructure according to standards set forth in FCC 10-36 Public Facilities.

FINDING: City of Florence Public Works has provided referral comments and noted that the provision of adequate public facilities can be provided, as conditioned. The applicant's proposed utility plan notes that *"electrical and franchised utilities are to be underground and designed to Central Lincoln People Utility District, Charter Communications, Century Link, etc, standards."* Approval will be shown on the utility plan (**Condition 1**).

L. Requiring a time period within which the proposed use or portions thereof shall be developed.

FINDING: A timing plan for site development and public improvements related to Phase 1 of the PUD will need to be provided and approved by the Planning Director prior to the issuing of any building permits within Phase 1. (**Condition 9**)

M. Requiring bonds to insure performance of special conditions. (Ord. 625, 6-0-80)

FINDING: In the event that the applicant requests flexibility with regard to site developments including (but not limited to) the construction of street improvements (sidewalks, street trees, landscaping), the City may require the developer/applicant to provide bonding, a letter of credit, or other performance guarantee to ensure completion of required public improvements in a sum approved by the City as sufficient to cover the costs of the improvements and repairs, including related engineering and incidental expenses, and to cover the cost of City inspection. (**Condition 16**).

10-7 SPECIAL DEVELOPMENT STANDARDS (MARCH, 2008) (same as current)

10-7-3: DEVELOPMENT STANDARDS

A. Special Flood Hazard Area: All uses proposed in the flood area shall conform to the provisions of the National Flood Insurance Programs.

FINDING: The proposed development area is located outside of the 100-year base flood elevation. Therefore, this criterion is met.

- B. Munsel Creek and Other Drainageways: A fifty foot (50') setback shall be required for all buildings from the creek channel, except by Planning Commission approval where it can be shown by accepted engineering practices or treatment that no erosion hazards, slide potential, or possible flood damage are likely to occur, and that riparian vegetation will be protected.**

FINDING: The development area is proposed to be located more than 50 feet from Munsel Creek and the Siuslaw Estuary.

- C. Foredunes: No grading or breaching of foredune is permitted.**

FINDING: The site investigation report identified that there are no “active foredunes” present on the site itself. No grading or breaching of foredune is permitted.

- D. River Cutbanks: No building shall be permitted within fifty feet (50') of a river cutbank unless the bank has been stabilized. Planning Commission approval, upon review of findings from site investigation report, is necessary for a lesser distance, using the same criteria as for Munsel Creek.**

FINDING: The site is not located next to a river cutbank as identified on the City of Florence Hazards Map which indicates the locations of river cutbanks, and these are not located near or adjacent to the subject property. Therefore, this criterion is met.

- E. Active Dune Advancing Edge: No building shall be permitted within one hundred feet (100') of the leading edge of an active dune.**

FINDING: The site does not include any active advancing dunes, nor is it located adjacent to any such dune, therefore, this criterion does not apply.

- F. Ocean Flooding Tidal Flooding, Tsunami: (See subsection A above, Special Flood Hazard Area).**

FINDING: The proposed development area is located outside of the 100-year base flood elevation, and the Florence Tsunami Hazard Area. Therefore, this criterion is met.

- G. Slopes Greater than Twelve Percent: For development on steep slopes, a foundation design and grading with provision for retaining walls or excavated banks shall be carried out according to plans prepared by a registered engineer and approved by the City.**

FINDING: There are no slopes in excess of 12% on the site.

- H. Active Dune Sands: Open sand will require primary stabilization as with European beach grass and secondary stabilization with any of a variety of shrubs and trees in conjunction with any development. Stabilization may be required prior to development in cases where there are large unstabilized areas.**

FINDING: The site does not include any dunes, nor is it located adjacent to a dune, therefore, this criterion does not apply.

- I. Brallier and Heceta Soils: In general these soils are not suitable for development. Should development occur, structures would be built on pilings or fill as designed by a registered engineer.**

FINDING: The site does not include any Brallier and Heceta soils, therefore, this criterion does not apply.

- J. Yaquina Soils and Wet Areas: In areas with seasonal standing water, construction of a drainage system and/or placement of fill material shall be required according to plans prepared by a registered engineer and approved by the City.**

FINDING: Yaquina Loamy Fine Sand is deep, excessively drained soil located on stabilized sand dune and 14 percent of the site is identified as Yaquina soil. Verification of soils bearing capacity and a grading plan, including storm-water design is required, and must be prepared by a registered engineer prior to construction activities.

10-7-4: SITE INVESTIGATION

- A. Areas Requiring a Site Investigation: Areas identified on the "Hazards Map", "Soils Map", or Resource Inventory are subject to the site investigation procedure contained in site investigation reports by Wilbur E. Ternyik, published by OCZMA. No building permit, conditional use permit or other permit subject to the provisions of this Title may be issued except with affirmative findings that:**

- 1. Upon specific examination of the site, the condition identified on the "Hazards Map" or "Soils Map" or supporting inventory documents did not exist on the subject property; or**
- 2. That harmful effects could be mitigated or eliminated through, for example, foundation of structural engineering, setbacks or dedication of protected natural areas.**

Site investigation requirements may be waived where specific standards, adequate to eliminate the danger to health, safety and property, have been adopted by the City. This exception would apply to flood-prone areas, which are subject to requirements of the National Flood Insurance Program and other problem areas which may be adequately protected through provisions of the Building Code. (Ord. 669, 5-17-82)

FINDING: A site investigation report was conducted during the Preliminary PUD process (Exhibit 45) and the only hazard identified is an earthquake hazard, which covers all of Florence and its environs. To mitigate potential risk from an earthquake, the commercial use buildings will be required to address designs which include seismic provisions of OSSC 1623 or ASCE-7 with appropriate criteria for the site class and location and verification of soils bearing capacity.

10-3 OFF-STREET PARKING & LOADING (MARCH, 2008)

10-3-2: GENERAL PROVISIONS:

- A. The provision for and maintenance of off-street parking and loading spaces are continuing obligations of the property owners. No building or other permit shall be issued until plans are presented that show property that is and will remain available for exclusive use as off-street parking and loading space.**
- B. At the time of new construction, enlargement or change in use of an existing structure within any district in the City, off-street parking spaces shall be provided as outlined in this Chapter, unless requirements are otherwise established by special review or City Council action.**
- C. If parking space has been provided in connection with an existing use or is added to an existing use, the parking space shall not be eliminated if elimination would result in less space than is required by this Chapter.**
- D. Required parking spaces shall be available for the parking of passenger automobiles of residents, customers, patrons and employees, and shall not be used for storage of materials of any type. (Ord. 625, 6-30-80)**
- E. Ingress and egress for parking and loading shall not endanger or impede the flow of traffic.**
- F. The required off-street parking for nonresidential uses shall not be used for loading and unloading operations during regular business hours.**
- G. For nonresidential uses, a portion of a parking lot, not to exceed a maximum of thirty percent (30%) of the total number of spaces may be reserved for compact and/or subcompact vehicles only. For purposes of this Section, the following definitions shall apply:**
 - 1. Compact/Subcompact Vehicle: Any vehicle which has a turning diameter of thirty six feet (36.0') or less.**

2. Standard Size Vehicle: Any passenger vehicle which has a turning diameter of more than thirty six feet (36.0).

H. Provisions of this Chapter shall not apply to any property located in an organized parking district. (Ord. 669, 5-17- 82)

FINDING: There are no specific concerns related to ingress and egress related to parking. The applicant's site plan includes a parking dynamic which does not appear to fundamentally threaten the availability of spaces due to storage or potential loading and unloading activities. A number of provisions within the General Provisions (10-3-2) section address elements of future construction or possible alterations. **Condition 6** will ensure that these provisions are observed and enforced as they existed in March 2008, unless the Planning Director, Planning Commission or City Council choose to apply current code provisions instead.

10-3-3: MINIMUM SPACES REQUIRED: Where square footage is specified, the area measured shall be the combined floor area on each level of a building exclusive of private office space, walk-in coolers, vent shafts, courtyards, stairwells, elevator shafts, restrooms, rooms designed and used for the purpose of storage and operation of maintenance equipment, and covered or enclosed loading docks. The number of employees shall include those working on the premises, plus proprietors.

During the largest shift at peak season, fractional space requirements shall be counted as the next highest whole space.

FINDING: To reduce the impact of parking areas, shared parking lots are planned to facilitate the needs of residents, employees and patrons of the commercial uses. To reduce the cost of housing and to recognize the mix of uses will decrease the reliance on the automobile, residential units will be designed to emphasize the quality and size of the living space versus the area set aside for a car. The Cannery Station site plan illustrates the anticipated level of density and the ability to meet required off-street parking. (Exhibit 2, Sheet A1.1 Site Plan).

The City Code parking standard has been updated since the original application was submitted. Therefore, Title 10, Chapter 3 Off-Street Parking and Loading standards in effect on March 28, 2008 (date PUD application was submitted) are used to evaluate the Preliminary PUD and remains applicable for the Final PUD.

The PUD chapter of the code specifically grants the Planning Commission the authority to reduce the required parking for single-family homes. To address this, Condition 7 from the Preliminary PUD requires a minimum of one covered parking space per single-family home on lots 3 & 4 and through 14 through 31 (per FCC 10-23-5-E) is allowed with the condition that: 1) the driveway is a minimum of 19 feet in depth to allow a second car to park on-site, and 2) the required covered parking space shall not be converted into a living space. Condition 7 applies to Lots 3 and 4 of proposed Phase 1 (formerly Lots 23 & 24 from the Preliminary development plan). Each single family home within the proposal has one covered garage space and a driveway space sufficient to accommodate a second vehicle.

Condition 8 of the preliminary PUD notes that if the cottage-style units on the assisted living campus (Lot 2) include a garage or carport, then a 19-foot minimum driveway length must be provided for the garage and a 3-foot maximum or 19-foot minimum driveway length must be provided where a carport is provided. In the applicant's statement, they note that any garage or carports associated with the cottage-style units on the assisted living campus will be designed to comply with the condition. They note that initially, the entrances to the cottages facing Spruce Street may be limited to pedestrian access until both sides of the street are fully improved and the wall is constructed between Florentine Estates and Cannery Station.

An enlarged floor plan shows a proposed medical office building for Lot 1 (Exhibit 29, Sheet C6.3). The conceptual building is 4,886 gross square feet. For the purpose of calculating the number of required parking spaces, and using the parking lot standards allowed for this project, 1,387 square feet can be deducted from the total square footage. The result is 3,499 square feet (subtracting private offices, restrooms, and storage areas). Using the one parking space per 200 square feet for medical office, 17.5 spaces are required. 18 spaces are proposed.

The applicant has provided a parking summary as part of Exhibit 41, and as "Updated PUD Development Data" on Exhibit 15, Sheet A1.1. The summary presents use and square footage calculations for the proposed conceptual development.

10-3-5: COMMON FACILITIES FOR MIXED USES:

- A. In the case of mixed uses, the total requirement of off- street parking space shall be the sum of the requirements for the various uses. Reductions from the minimum parking requirements for individual uses may be granted by the Design Review Board where circumstances indicate that joint use of parking or other factors will mitigate peak parking demand.**

Requests for parking reductions shall be made to the Design Review Board by filing an application for Design Review and shall be supported by information contained in a traffic engineering investigation, prepared by a registered traffic engineer. The investigation shall generally follow the format outlined below based upon the document "Shared Parking" authored by the Urban Land Institute.

1. Step One: Initial Project Review

Document and quantify the proposed land uses and anticipated functional interrelationships between differing uses. The initial phase also must include data gathered regarding general location of parking facilities, surrounding land uses, land use mix and other variables which affect parking.

2. Step Two: Adjustment for Peak Parking Factor.

Calculate the number of off-street parking spaces required for each land use within the study area.

3. Step Three: Analysis of Hourly Accumulation.

Estimate the hourly parking accumulations for each land use during a typical weekday and weekend day.

4. Step Four: Estimate of Shared Parking.

Combine the hourly parking demand for each land use to determine the overall parking to be required within the planning area.

FINDING: As the first proposed development on the site, Lot 1 does not propose and shared parking dynamic. However, to reduce the impact of parking areas in the development of other lots, shared parking lots are planned to facilitate the needs of residents, employees and patrons of the commercial uses. Any future development which proposes an element of shared parking will be required to provide the analysis outlined in 10-3-5 (A) sufficient to determine if the criteria are met. The applicant shall provide such information at the time of Design Review for each respective development.

B. In granting parking reductions, the Design Review Board shall make one or more of the following findings:

1. The traffic engineering report justifies the requested parking reduction based upon the presence of two or more adjacent land uses which, because of substantially different operating hours or different peak parking characteristics, will allow joint use of the same parking facilities.

2. The traffic engineering report indicates the presence of public transportation facilities and/or pedestrian circulation opportunities which justify the requested reduction of parking.

3. The traffic engineering report finds that the clustering of different land uses is such that a reduced number of parking spaces can serve multiple trip purposes to the area in questions.

C. As a condition of approval to the granting of a parking reduction, the City may require the recording of reciprocal access and parking agreements between affected property owners.

D. The parking facility for which shared parking or off-site parking is proposed shall be no further than one thousand feet (1000') from the building or use required to provide parking. The Design Review Board, upon submittal of a Design Review applications, may grant approval for off-site parking only if affirmative findings can be made that:

1. The location of the parking facility will not be detrimental to the safety and welfare of residents in the area; and,
2. Reasonably safe pedestrian access will be provided from the parking facility to the building or use requiring the parking; and,
3. The property owner of land for which a building or use requires off-site parking has recorded a covenant agreeing to require any occupant or tenant to maintain such parking facilities; and,
4. The applicant requesting off-site parking has furnished a copy of a recorded exclusive, perpetual easement granting to the property owner of the land for which the off-site parking is to be located, use of the off-site property for parking purposes in perpetuity.

E. Any decision of the Design Review Board may be appealed to the City Council in accordance with the procedures specified in Code Section 10-1-1-6. (Ord 19 Series 1994).

FINDING: No parking reductions are requested or proposed for Lot 1. The provisions of 10-3-5 are reflected in **Condition 6**, by requiring that approval of plans for parking improvements which adequately address FCC 10-3-5 prior to Design Review approval of any proposal which includes shared parking.

10-3-6: PARKING AREA IMPROVEMENT STANDARDS:

All public or private parking areas, loading areas and outdoor vehicle sales areas shall be improved according to the following: All required parking areas shall have a durable, dust free surfacing of asphaltic concrete, cement concrete or other materials approved by the City. (Ord 14 Series 1995)

- A. Parking for new single family dwellings shall be provided as a carport or garage, unless the majority of existing dwellings within 100 feet of the property boundary of the proposed development do not have such covered parking facilities. The number of required covered parking spaces shall be based on the predominant number of covered spaces on the majority of lots within the 100 foot radius. (Ord 12 Series 1994, 7-5-94)
- B. All parking areas except those required in conjunction with a single-family or duplex dwelling shall be graded so as not to drain storm water over public sidewalks. All drainage systems shall be connected to storm sewers where available. Parking lot surfacing shall not encroach upon a public right of way except where it abuts a concrete public sidewalk, or has been otherwise approved by the City.
- C. Except for parking areas required in conjunction with a single-family or duplex dwelling, all parking areas shall provide:

1. A curb of not less than six inches (6") in height near abutting streets and interior lot lines. This curb shall be placed to prevent a motor vehicle from encroaching on adjacent private property, public walkways or sidewalks or the minimum landscaped area required in paragraph C2 of this subsection.
 2. Except for places of ingress and egress, a three foot (3') landscaped area wherever it abuts street right-of-way. In areas of extensive pedestrian traffic or when design of an existing parking lot makes the requirements of this paragraph unfeasible, the Planning Commission may approve other landscaped areas on the property in lieu of the required three foot (3') landscaped area.
- D. No parking area shall extend into the public way except by agreement with the City.
- E. Except for parking in connection with dwellings, parking and loading areas adjacent to a dwelling shall be designed to minimize disturbance by the placement of a sight obscuring fence or evergreen hedge of not less than three feet (3') nor more than six feet (6') in height, except where vision clearance is required. Any fence, or evergreen hedge must be well kept and maintained.
- F. Any lights provided to illuminate any public or private parking area or vehicle sales area shall be so arranged as to reflect the light away from any abutting or adjacent residential district.
- G. Except for single-family and duplex dwellings, groups of more than two (2) parking spaces shall be so located and served by a driveway that their use will require no backing movements or other maneuvering within a street right of way other than an alley.
- H. Unless otherwise provided, required parking and loading spaces shall not be located in a required front or side yard.
- I. Building permits are required for all parking lot construction or resurfacing.
- J. A plan, drawn to a suitable scale, indicating how the off- street parking and loading requirements are to be met shall accompany an application for a building permit. The plan shall indicate in detail all of the following:
1. Individual parking and loading spaces.
 2. Circulation area.
 3. Access to streets and property to be served.
 4. Curb cut dimensions.

5. Dimensions, continuity and substance of screening, if any.
6. Grading, drainage, surfacing and subgrading details.
7. Obstacles, if any, to parking and traffic circulation in finished parking areas.
8. Specifications for signs, bumper guards and curbs.
9. Landscaping and lighting.

K. Other pertinent details.

L. In addition to other penalties and remedies, the failure to provide, maintain and care for a parking area as required by this Section:

1. Is declared a public nuisance which may be abated under subsection 6-1-8-5 of this Code.
2. May be the basis for denying any business license required or permit issued by the City. (Ord. 625, 6-30-80; re-lettered by Ord. 669, 5-17-82; Ord. 4, Series 1985, 4-23- 85)

FINDING: Parking improvement construction specifications have not been provided by the applicant to a detail that permits an evaluation of these criteria at this time (including for Lot 1). The provisions of 10-3-6 are reflected in **Condition 6**, by requiring approval, by the Planning Director, of construction plans for parking improvements which adequately address FCC 10-3-6 prior to issuance of any building permit.

10-3-7: OFF-STREET LOADING:

- A. All necessary loading spaces for commercial and industrial buildings and uses shall be off the street and shall be provided in addition to the required parking spaces.
- B. Vehicles in the berth shall not protrude into a public right of way or sidewalk. When possible, loading berths shall be located so that vehicles are not required to back or maneuver in a public street.
- C. A school having a capacity greater than twenty five (25) students shall have a driveway designed for continuous forward flow of passenger vehicles for the purpose of loading and unloading children.

FINDING: These criteria are met with the Phase 1 final PUD proposal

10-3-8: PARKING TABLE AND DIAGRAM:

FINDING: The applicant has provided a parking summary as part of Exhibit 41, and as "Updated PUD Development Data" on Exhibit 15, Sheet A1.1, Land Use Plan.

The summary presents use and square footage calculations for the proposed conceptual development. The table and diagram propose a range of parking scenarios which are consistent with FCC Chapter 10.

10-3-9: PARKING SPACE DIMENSIONS:

- A. Parking stalls shall be a minimum of nine and one-half feet wide and nineteen feet in length (9 1/2' x 19'), with double line striping, two feet (2') wide on center. (Ord. 625, 6-30-80)**
- B. Parking spaces for compact cars shall be a minimum of seven and one-half feet wide and seventeen feet in length (7 1/2' x 17') with double line striping, two feet (2') wide on center.**
- C. The width of any striping line used in an approved parking area shall be a minimum of 4" wide. (Ord. No. 2, Series 2000)**

FINDING: The Lot 1 Civil Site Plan (Exhibit 29, Sheet C6.3) provides dimensions for parking spaces proposed for the site, and relative to other areas proposed in Phase 1. The spaces are 9.5 feet wide and 19 feet in depth.

10-30 NORTH COMMERCIAL NODE (MARCH, 2008)

10-30-6: DESIGN CRITERIA

Facades: Building facades shall be articulated to avoid long, unbroken surfaces. This may be accomplished by varying the setback of the building façade, adding tower elements to the building, providing bay windows or covered walkways, or providing other vertical or horizontal structural treatments to the building façade.

- B. Roofs:** Hipped or gabled roofs are recommended. Flat roofs are permitted only if the roof line is either broken up with vertical treatments such as tower elements or decorative parapets and cornice treatments are provided. Mansard roofs are not permitted. HVAC equipment and other roof-mounted equipment shall be adequately screened or hidden from view from adjacent streets and property.
- C. Materials:** Exterior materials on buildings shall be masonry, horizontal wood siding or shingles, stucco or similar material. Sheet metal and cinder block are discouraged as primary exterior wall material. T1-11 may be used in conjunction with horizontal siding if approved by the Design Review Board.
- D. Colors:** Predominant roof and building colors shall be consistent with the City's desired coastal village atmosphere. Compatible colors consist of

earth tones and soft pastels. Incompatible roof and body colors include any colors used where the intent is to attract attention instead of complementing and accentuating the building design.

FINDING: The Design Review criteria evaluated in this staff report address each of design criteria noted above. The findings and conditions from Design Review ensure that the Design Criteria for the North Commercial Node District are met.

VIII. CONCLUSIONS AND RECOMMENDATIONS

Staff finds that the proposed Final PUD for Phase 1, and accompanying Preliminary Subdivision Plan and Design Review for Cannery Station PUD meets or can meet the requirements of the City Code with conditions, and recommends APPROVAL of the Final PUD, Preliminary Subdivision, and Design Review subject to the following conditions. Note that these are preliminary conditions and more will be added based on the input from the public hearing and direction from the Planning Commission.

1. Approval shall be shown on:

Exhibit 1: Findings of Fact

Exhibit 2: Land Use Application Narratives (FPUD and Preliminary Subdivision).

Exhibits 15-36: Land Use Application Exhibits

Exhibit 53: Extent of Wall

Findings of Fact attached as Exhibit "1" are incorporated by reference and adopted in support of this decision. Any modifications to the approved plans or changes of use, except those changes relating to the interior regulated by Building Codes or those needed to comply with conditions of approval, will require approval by the Community Development Director or Planning Commission/Design Review Board. All required site improvements shall be made prior to final inspection and occupancy the City agrees to a performance agreement and financial security.

Final PUD Conditions (2-9):

2. Before installation of any public improvements on any proposed lots east of proposed Redwood Street has begun, the 8-foot wall between Florentine Estates and the proposed development shall be constructed in it's entirety, unless otherwise conditioned by the Planning Commission.
3. Before installation of any public improvements on any proposed lots east of proposed Redwood Street has begun, the 8-foot wall between Florentine Estates and the proposed development shall be constructed in it's entirety as shown in Exhibit 53, unless otherwise conditioned by the Planning Commission.

4. The Final PUD (Phase 1) contains vehicular turn-around areas at the temporary terminus of Spruce and Redwood Streets (See Exhibit 26, Sheet C6.0, Street Plan). These turn-around areas will function for fire and emergency medical and other motor vehicles. As the streets are further extended, the interim turn-around areas must be shifted. If only a segment of 47th Street is constructed, the Final PUD application must include a temporary turn-around area for motor vehicles.
5. Prior to construction of the cantilevered cover for the senior housing facility drop-off, the developer/applicant will need to secure/enter into an agreement with the City of Florence for the private use of the public right-of-way.
6. The applicant shall provide a complete Lighting Plan for public improvements for Phase 1 of the Cannery Station PUD, and a lighting plan for the on-site improvements of Lot 1, and obtain approval of the plans from the Florence Planning Director before building permits are obtained.
7. Unless the Planning Director, Planning Commission or City Council choose to apply current code provisions, the off-street parking provisions of FCC 10-3-2 GENERAL PROVISIONS, 10-3-6 PARKING AREA IMPROVEMENT STANDARD and 10-3-7 OFF-STREET LOADING (as constituted in March, 2008, and included in the findings of fact (Exhibit 1) for PC 12 12 FPU D 01, shall be observed and receive necessary approval for Phase 1 PUD development prior to the issuance of any buildings permits for Cannery Station. This includes the submission of construction plans for parking improvements on Lot 1 which adequately address FCC 10-3-6.
8. To ensure that construction activity for the wall does not encroach upon the existing landscape buffer between Florentine Estates and Cannery Station, prior to any development activity, the developer shall clearly mark, with orange construction fencing, the perimeter of the area of impact.
9. Any future proposed development within the Phase 1 PUD which includes shared parking improvements must adequately address FCC 10-3-5 prior to Design Review approval
10. A timing plan which outlines when improvements for Phase 1 of the Cannery Station PUD are envisioned to be completed must be provided to the Planning Director and approved prior to the issuing of any building permits within Phase I.

Preliminary Subdivision Conditions (10-17):

11. The proposed water line must be sized in accordance with the Oregon Fire Code and City of Florence and Siuslaw Valley Fire and Rescue adopted amendments. Minimum fire flow will be determined at the time of building permit submittal.
12. Prior to Final Subdivision Plat Approval for any development abutting 47th Street (unless ODOT determines it is needed sooner), applicant shall construct a new public street connection to Highway 101 (47th Street)

approximately 992 feet south of Munsel Lake Road. Before street improvements begin, the applicant shall submit a revised Sheet C6.0 that illustrates the ODOT improvements required based on the new road approach permit required as per ODOT referral comments dated June 24, 2013 (Exhibit 48).

13. Prior to Final Subdivision Plat approval of those areas affected by a wetland, the applicant shall 1) have acquired all required permits, such as removal-fill permit and/or drainage into wetlands from the Department of State Lands (DSL), and from US Army Corps of Engineers (USACOE), and 2) record a deed restriction to prevent current and future impacts to wetland areas with Lane County Deeds and Records and provide a copy to the City.
14. The preliminary subdivision plan does not include a 35-foot setback line from the eastern property line. The setback must be shown on the final subdivision plat to the extent permissible by Oregon Revised Statute 92.
15. The reserve strip at the south end of Spruce Street (adjacent to the southern property line) will be widened to span the entire distance of the 50-foot right-of-way.
16. The applicant will be expected to proceed with final survey and to make preparations for final subdivision approval within the timeframes outlined in FCC 11-3-6, which will necessitate that the applicant submit for Final subdivision plat within 2 years of the date of preliminary plan approval, (May 28, 2015) unless otherwise provided for through approved and allowed extensions from the Planning Commission.
17. In the event that the applicant requests flexibility with regard to site developments including (but not limited to) the construction of street improvements (sidewalks, street trees, landscaping), the City may require the developer/applicant to provide bonding, a letter of credit, or other performance guarantee to ensure completion of required public improvements in a sum approved by the City as sufficient to cover the costs of the improvements and repairs, including related engineering and incidental expenses, and to cover the cost of City inspection.
18. Before approval of the Phase 1 Final Subdivision Plat, the applicant will submit verification that ODOT has issued the approach permit necessary for 47th Street access at Highway 101.

Design Review Conditions (18):

19. Prior to initiating site development, the applicant shall contact the Confederated Tribes of Coos, Lower Umpqua, and Siuslaw Indians at a least 72 hours' notice prior to any ground disturbing activities, so a tribal representative can be present during these activities; and if any known or suspected cultural resources are encountered during any phase of the project, contact the Tribes immediately.

Final PUD Informational Items

- Prior to issuance of a Certificate of Occupancy for each development phase, applicant shall have mitigated associated traffic impacts as determined and approved by ODOT. The design and construction of the proposed improvements to mitigate the traffic impacts must be approved by ODOT and be consistent with the requirements of the approved permits.
- An NPDES General Permit 1200-C Application has been submitted to DEQ as part of the final Phase 1 PUD (Refer to Exhibit 11, NPDES Permit). The approved NPDES plan and permit will be required prior to any site development.
- Buildings, including any commercial structure, apartment complex and the assisted living center, on the Cannery Station site will be required to have an NFPA 13 sprinkler system installed as per Siuslaw Fire and Rescue Marshall, Sean Barrett. If single family units are town houses, apartments or other multifamily dwellings they will/may require at least a 13R system depending on construction and code version used. Additionally, the district requires a fire hydrant within 50 feet of the FDC. For proper coverage, with approval from the City of Florence Public Works Director, the fire district requests the provision of fire hydrants at the entrance of 47th Street on the south side and on east the apartment entrance on the north side of 47th Street (in addition to other proposed hydrants).
- The City Comprehensive Plan requires two northbound lanes to ultimately be constructed along the frontage, including a right turn deceleration lane for the entrance into the project from Hwy 101 and at Munsel Lake Road. Currently the two north bound lane section terminates prior to 42nd street and merges into one north bound travel lane. In order to comply with the City Transportation System Plan (TSP), the City will eventually require the developer to widen the roadway and install curbs/sidewalks to the ultimate alignment in front of the development. ODOT referral comments for future phases will inform the City's expectations for Highway 101 improvements. The City is working to add the lane widening to the Street System Development Charge (SDC) calculations so that it is a reimbursable expense. The developer will not be required to strip the second lane. Striping of the lane(s) will take place at a time and date in the future when the rest of the improvements (adding the second north bound travel lane from south of 42nd street) are completed. If the City is unsuccessful in updating the Street SDC list, then the City reserves the right for a non-remonstrance agreement.
- In its letter dated June 24, 2013, ODOT has recommended the applicant resubmit *An Application for State Highway Approach* (road approach permit application) for the 47th Street connection. The application will be subject to review and approval criteria in OAR 734-051. There is no application fee. It should be noted approval of the application may result in highway improvements or conditions that at this time may not be

identified. Any improvements within state highway right-of-way will require submittal of construction drawings that must be reviewed and approved by ODOT prior to the issuance of a Construction Permit per OAR 734-051-5020.

- If the cottage-style units on the assisted living campus (lot 2) include a garage or carport, the following driveway standards, measured from the property line shall apply:
 - Garage: 19-foot minimum driveway length; and
 - Carport: 3-foot maximum or 19-foot minimum driveway length, and where carports are located 3 feet from the property line, the carport shall not have walls or doors forward of the building setback line. (A carport is an open-sided automobile shelter.)

Subdivision Informational Items

- At the time of construction, construction drawings shall be submitted and approved by the Public Works Director. Such drawings shall include the following information:
 - Show profiles of all utilities (sanitary sewer, stormwater and water)
 - Show all service lateral locations, including water meters, Fire Department Connection (FDC) and Double Detector Check Valve (DDCV) assembly locations
 - Field verify sanitary sewer rim and invert elevations on Hwy 101
 - Show street light design and locations
 - Show standards for all utilities (sewer, water, stormwater, street lighting).
 - Street specifications showing that Redwood Street will be designed to handle the size and weight of any proposed transit vehicle expected to be used with consideration to the annual number of trips to be made by the service
- Maintenance of the Highway 101 right-of-way must be coordinated between the applicant and ODOT as the City will not be responsible for maintaining the Highway 101 right-of-way.

Design Review Informational Items

- Prior to issuance of a building permit, a soils report and verification of soils bearing capacity and a grading plan, including storm-water design is required. The plan must be prepared by a registered engineer.
- Building permit requirements will include submittal of detailed construction plans showing compliance with Oregon "Specialty" (Building, Plumbing, Electrical, and Mechanical) Codes as appropriate to the use, including the Oregon Fire Code requirements for access and water supply for "Fire Flow" (OFC Appendices B & D, etc.). All "R"

(residential) and R/mixed use occupancies require automatic fire suppression systems per Chapter 9 of the OSSC. Commercial use buildings require seismic detailing provisions of OSSC 1613 or ASCE-7 with appropriate criteria for the site class and location. The Siuslaw Valley Fire District and Building Department must approve access, fire department connections, and installed fire sprinkler systems. The buildings and site shall meet Americans with Disabilities Act and Oregon accessibility requirements.

- The wall proposed between the subject site and Florentine Estates (to the East) will require Design Review approval.
- As per FCC 10-6-9, authorization of a design review permit shall be void one (1) year after the date of approval of a design review application, unless a building permit has been issued and substantial construction pursuant thereto has taken place (building foundation). The applicant may apply for a one-time extension of one (1) year maximum duration based on compliance with the following criteria:
 - The request for an extension is made in writing prior to expiration of the original approval.
 - There are special or unusual circumstances that exist which warrant an extension.
 - No material changes of surrounding land uses or zoning has occurred.

IX. EXHIBITS

1. *Findings of Fact*
2. *FPU D Land Use Application/Compliance Statement*
3. *Subdivision Application/Compliance Statement*
4. *Subdivision Preliminary Plan*
5. *Aerial Photo*
6. *Sewer Detail*
7. *Storm Water Detail*
8. *Water Detail*
9. *Joint Permit Application/Joint Permit Application Signature*
10. *Lot 1 exterior Materials and Colors*
11. *NPDES 1200 – C Application/NPDES EC3.0*
12. *Draft Declaration of Covenants, Conditions and Restrictions (CC&Rs)*
13. *City of Florence Incompleteness Letter (May, 2012)*
14. *Written Agreement with Florence Public Works for Service*
15. *A1.1. Land Use Plan*
16. *A1.2 Open Space Plan*
17. *A2.1 Lots 1 & 6 (Buildings)*
18. *A2.2. Lot 2 (Buildings)*
19. *A2.3 Lots 3-5 (Buildings)*
20. *A2.4 Lot 1 (Buildings)*

21. C1.0 Existing Conditions Plan
22. C2.0 Mass-Grading Plan
23. C2.1 Grading Plan (Lot 1)
24. C3.0 Utility Plan
25. C3.1 Lot 1 Utility Plan
26. C6.0 Street Plan
27. C6.1 Typical Sections
28. C6.2 Street Intersection Detail/Sections
29. C6.3 Lot 1 Civil Site Plan
30. EC1.0 Erosion Control
31. EC2.0 Erosion Control Plan
32. EC3.0 Erosion Control Details
33. LA-1 Aerial
34. LA-2 Landscape Master Plan
35. LA-3 Plant Lists and Details
36. L-1 Lot 1 Landscape Plan
37. Supplemental Letter from Applicant (June 18, 2013)
38. Access Analysis for Cannery Station (Sandow Engineering) (June 18, 2013)
39. Affidavit of Services
40. Resolution PC 08 09 PUD 01 (Cannery Station Preliminary PUD Conditions)
41. Parking Data
42. Title Report/Legal Description
43. Modifications--Sidewalk Detail from Preliminary PUD
44. Email correspondence with Mike Miller Public Works Director (May 13 and 15, 2013)
45. Site Investigation Report from Preliminary PUD.
46. City of Florence Water Improvements Construction Bid (excerpt)
47. Florence Public Works and Siuslaw Valley Fire and Rescue Joint Statement on Cannery Station Water Service (July 2, 2013)
48. ODOT Referrals Comments (June 24, 2013 (revised) and May 28, 2013 (original))
49. Applicant e-mail correspondence related to water service (July 2, 2013)
50. Water service statement from applicant's civil engineer, Matt Keenan, kpff-Consulting Engineers (July 2, 2013).
51. Letter from Jason Nelson, Florentine Estates Facilities Operation Manager, (July 1, 2013).
52. Applicant's civil engineer's e-mail response to Florentine Estates Facilities Operation Manager, Jason Nelson's, letter (dated July 2, 2013)
53. Extent of Wall

2
Exhibit

PC1212 FPUD01
File Number

CITY OF FLORENCE
LAND USE APPLICATION
Community Development Department
250 Highway 101, Florence, OR 97439
(541) 997-8237 voice (541) 997-4106 fax



TYPE OF REQUEST

- Minor Design Review (DR) (less than 10,000 sq ft)
- Planned Unit Development (PUD)
- Zone Change (ZC)
- Major Design Review (DR) (more than 10,000 sq ft)
- Conditional Use Permit (CUP)
- Comprehensive Plan Amendment (CPA)
- Modification to approved plan
- Subdivision (SUB)
- Administrative Review (AR)

APPLICANT INFORMATION

Name: Archie & Company, Teresa Bishow Phone: 541-344-5500
E-Mail Address: teresa@archie.com Cell Phone: _____
Address: 2911 Tennyson Avenue Suite 400 Eugene, OR 97408
Signature: [Signature] Date: 5/17/2012
Applicant's Representative (if any) _____

PROPERTY OWNER INFORMATION

Name: Cannery Station, LLC Phone: 541-344-5500
E-Mail Address: _____ Cell Phone: _____
Address: 2911 Tennyson Avenue Suite 400 Eugene, OR 97408
Signature: [Signature] Date: 5/17/2012
Applicant's Representative (if any) _____

If applicant and property owner are not the same, either sign or submit a letter of authorization to allow the applicant to act as agent for the property owner.

PROPERTY DESCRIPTION

Property Address: 87344 Munsel Lake Road
General Location (Example: southwest corner of Second St and Hwy 101) southeast corner of
Hwy 101 and Munsel Lake Road
Map and Tax Lot Number: 18-12-14-2 TL 700 Lot Size 16.9 acres
Zoning Designation: North Commercial District / NCD

CONTINUED ON NEXT PAGE

**Compliance Statement
Cannery Station – Florence, Oregon
Phase 1 Final Planned Unit Development & Design Review**

I. OVERVIEW

A. Summary of Request

This application is the first Final PUD for Cannery Station and will go by the name *Cannery Station, Phase 1 Final PUD*. This application requests City approval of the proposed improvements:

- Lot #1- A one story 4,325 to 5,000 square foot commercial building and associated parking and landscape areas.
- Lot #2- An assisted living campus consisting of a three story building and eight cottages providing between 75 and 82 housing units. The lot will also contain open space and a parking area.
- Lot#3- A single attached cottage associated with the assisted living campus.
- Lot #4- A single attached cottage associated with the assisted living campus.
- Lot #5- A three story building for 30 to 50 apartments and associated parking and landscape areas. The range in apartments varies to allow for potential ground floor commercial space.
- Lot #6- A one story 4,000 to 4,600 square foot commercial building and associated parking and landscaping areas.
- Open Space Tract A located in the southwest corner of the development site designed as part of the stormwater management plan and an area for native plants.
- Open Space Tract B located in the southeast corner of the development site designed as part of the stormwater management plan and an area for native plants.
- Open Space Tract C located at the eastern terminus of 47th Street designed as a pocket park.

The Cannery Station, Phase 1 Final PUD also contains improvements to the southern segment of Highway 101, new local streets, and the extension of public utilities.

B. Applicable Zoning and Development Standards

Cannery Station is zoned North Commercial District. The Phase 1 Final PUD demonstrates compliance with standards applicable to property zoned North Commercial. In accordance with Resolution PC 08 09 PUD 01 adopting the preliminary PUD, the Phase 1 Final PUD also demonstrates compliance with applicable regulations and requirements of the Florence City Code in effect as of March 28, 2008 unless modified during the preliminary PUD.

C. Cannery Station Phasing Plan

As part of the Cannery Station preliminary PUD approval, the City approved a Phasing Plan allowing for Final PUD, Subdivision, and Design Review applications to be submitted over a ten year period from the date of preliminary PUD approval. As indicated on the Phasing Plan, the order of phases “does not denote or infer the order or actual timing of the development of the PUD...At each phase, further review will occur concerning the logical extension of urban services...” The intent of the Phasing Plan was to provide the framework for designing and constructing improvements in a sequential manner and avoid unnecessary site changes in advance of actual development. The Phasing Plan recognizes that in some cases, “certain utilities may need to be extended through part of the undeveloped portion of the site to serve the phase under construction.”

II. SITE INFORMATION

Location: Cannery Station is located at the southeast corner of Munsel Lake Road and Highway 101 in north Florence. The 17 acre site has frontage along the east side of Highway 101 and the south side of Munsel Lake Road. This Phase 1 Final PUD is for the southern portion of the site.

Tax Lot: Assessor Map 18-12-14-20, Tax Lot 700.

Acreage: Phase 1 consists of about 6.7 acres.

Address: 87344 Munsel Lake Road, Florence, Oregon 97439

Ownership: The site is owned by Cannery Station, LLC. Arlie & Company is the authorized representative.

Surrounding Land Uses: Florentine Estates, a retirement community, is located immediately to the east of the subject property. A church is located to the south of Cannery Station. Fred Meyer and other commercial uses are located across from the property on the west side of Highway 101. The property to the north is currently vacant.

Exhibits are attached to show the area surrounding the Property. The aerial photo shows property lines, existing streets, and natural features. Three additional exhibits show major utilities. The surrounding topography shows no significant grade changes. The detailed topography on the Existing Conditions Plan shows a 14 to 20 foot ridge that runs from the southeast to the northwest and can be seen from Highway 101. There are no known future streets planned other than those shown within the Plan and the changes proposed for Highway 101. *Please refer to exhibits C, D, E, F, Sheet C1.0 Existing Conditions Plan, and C6.0 Street Plan.*

Florence Plan Land Use Designation: The property is designated in the City of Florence Comprehensive Plan as North Commercial Node (NCN). The NCN is located around the four corners of the intersection of Highway 101 and Munsel Lake Road.

Zoning: The property is zoned North Commercial District.

III. FINAL DEVELOPMENT PLAN APPROVAL

A. Conformance with General Approval Criteria

Section 10-23-11 of the Florence City Code contains a list of items to address while reviewing a Final PUD.

According to Resolution PC 08 09 PUD 01, Condition of Approval #5 states that "Within twelve (12) months following this approval of the Preliminary PUD, the applicant shall file with the Planning Commission a Final PUD and Design Review application for Phase 1." The Planning Commission's action approving the Preliminary PUD went into effect on November 19, 2008. The decision was to be in effect for one year or until November 19, 2009. Due to difficult economic conditions, on November 16, 2009 the Florence City Council conducted a public hearing on a proposed ordinance granting an automatic two year extension to all approved preliminary subdivisions and PUD's. Following the public hearing, the Council adopted Ordinance No. 19, Series 2009 approving the extensions and declaring an emergency. Cannery Station preliminary PUD was granted an extension to November 19, 2011. The Florence Planning Commission approved a request for a one-time six month extension to May 19, 2012. This Phase 1 Final PUD is submitted within the mandated time frame.

As set forth in Section 10-23-11, the Phase 1 Final PUD contains conceptual plans for public improvements that were prepared by a Registered Engineer and submitted for approval by City staff including:

Storm drainage
Sewer and water utilities
Streets and pedestrian ways

The Cannery Station preliminary subdivision plan is submitted for review under separate cover.

The Cannery Station Phase 1 Final PUD is substantially consistent with the preliminary PUD. There are no material deviations that necessitate the need for an amendment to the preliminary PUD.

B. Conformance with Preliminary PUD Conditions of Approval

The preliminary PUD conditions of approval are listed below followed by a brief explanation of how the Phase 1 Final PUD compiles.

1. Approval shall be shown on:...

Condition #1 is met. Refer to Resolution PC 08 09 PUD 01.

2. Regardless of the content of material presented for this Planning Commission decision...; the Applicant agrees to comply with all regulations and requirements of the Florence City Code which are current on the date the application was submitted (March 28, 2008)...

Condition #2 is met. Phase 1 Final PUD is in substantial conformance with the preliminary PUD and standards applicable as of March 28, 2008.

3. Off-site roadway improvements shall include...
 - a. A new public all-way access street connection to Munsel Lake Road (Redwood Street)...
 - b. Dedication of right-of-way and roadway widening of Munsel Lake Road...
 - c. A new public right-in-right-out street connection to Highway 101 (47th Street) approximately 992 feet south of Munsel Lake Road. Note: This condition does not limit the developer from seeking ODOT approval for a street connection that provides greater access.
 - d. Roadway widening to Highway 101...
 - e. Urban style improvements...on Highway 101 and on Munsel Lake Road.

The applicant completed preliminary drawings for all of the above off-site improvements as reflected on the Final PUD site plan. The new public street connection to Munsel Lake Road (Redwood Street) will be constructed as part of a future phase. The dedication of needed right-of-way on Munsel Lake Road will occur as part of the subdivision process that creates any new lots adjacent to Munsel Lake Road. Improvements to Munsel Lake Road will be done as part of a future phase when development in Cannery Station occurs in the northern portion of the site.

On January 4, 2010, ODOT approved a new public street connection to Highway 101 (47th Street). On January 4, 2010 ODOT approved the Munsel Lake Road approach permit and set forth required mitigation measures. The off-site improvements shown on this Phase 1 Final PUD site plan illustrate the ultimate full build-out design in accordance with approved ODOT permits.

On September 9, 2010, ODOT extended the deadline for generating construction drawings for the 47th Street road approach permit (ODOT application number 7905). Subsequently, ODOT granted additional six month extensions. The current deadline is October 25, 2012.

On September 9, 2010, ODOT issued a letter stating that the approach permit for Munsel Lake Road is no longer required and will no longer be processed under Oregon Administrative Rule (OAR) Chapter 734, Division 51. The letter explained that on March 4, 2010, the governor signed Senate Bill 1024 into law. Senate Bill 1024 made changes to Oregon Revised Statute (ORS) 374.310 and changed ODOT's authority to manage highway access. According to the letter from ODOT, the new law eliminates ODOT's authority to require that a local jurisdiction obtain a Permit to Operate, Maintain, and Use a State Highway Approach for an existing or planned city street or county road connection to a state highway.

As part of this Phase 1 Final PUD, the southern portion of Highway 101 abutting the phase boundary will be widened and improved in compliance with the above condition. Conceptual designs for the interim Highway 101 improvements are attached. Please refer to the drawings dated May 8, 2012 prepared by JRH Transportation Engineering. The final designs are subject to ODOT approval.

4. Maintenance of the Highway 101 right-of-way must be coordinated between the applicant and ODOT...

Condition #4 will be addressed during ODOT's review of required permits for Highway 101.

5. Final PUD is authorized to be processed in phases along with each subdivision application (per FCC 10-23-10 last paragraph) with the following conditions:
 - a. Within twelve (12) months following this approval of the Preliminary PUD, the applicant shall file with the Planning Commission a Final PUD and Design Review application for Phase 1...

The Cannery Station preliminary PUD was granted an automatic two year extension to November 19, 2011. The Planning Commission later granted a one-time six month extension to May 19, 2012. This Final PUD and Design Review application was submitted within the required time frame.

- b. Phasing is not bound by sequence... with condition that the Final PUD and Design Review application for the last remaining phase shall be submitted within ten (10) years following this approval of the Preliminary PUD.

This Final PUD includes the southern portion of the site at the entrance on Highway 101 at the new 47th Street. It is a logical place to begin development and will not hinder the remaining phases from being submitted within ten years of the extended expiration of the Preliminary PUD or by May 18, 2022.

- c. At each phase of the Final PUD, the applicant shall demonstrate substantial compliance with the approved Preliminary PUD and all applicable conditions.

As demonstrated in this Final PUD, Phase 1 is in substantial conformance with the approved Preliminary PUD and all applicable conditions.

- d. At each phase of the Final PUD, the applicant shall demonstrate that all public services can be provided.

In April 2010, the City of Florence and Cannery Station LLC reached a mutual agreement on the how City services would be provided to the subject property in addition to improvements completed as part of the Spruce Street LID. The preliminary utility plan submitted with this Final PUD reflects the agreement on the method of providing public services to the site. *Please refer to Sheet C3.0 Utility Plan.*

- e. At each phase of the Final PUD, the applicant shall provide monitoring information and traffic data in order to assess needed mitigation of traffic impacts for that development phase, and propose appropriate mitigation for that development phase.

Cannery Station is currently vacant and there is no traffic being generated at the site. This condition is not applicable to this Phase 1 Final PUD. In future phases, traffic data may need to be collected to evaluate impacts for proposed additional developments.

- f. At each phase of the Final PUD, the applicant shall provide a supplemental analysis assuring operational safety and adequate geometric design of intersections shall be provided for on-site circulation and delivery truck as well as any proposed transit vehicle access.

This phase does not trigger the need for any supplemental analysis.

- g. At each phase of the Final PUD, the applicant shall provide a calculation of the square footage proposed for each of the land uses within the current phase, as well as the cumulative calculation of the square footage that has been achieved in any previously approved and/or developed phases. Any increases to the maximum land use intensities described above require approval from the Planning Commission and the applicant to submit new traffic calculations showing, as a result of the proposed changes that projected p.m. peak hour trips from the combination of the proposed uses will be less than or equal to 589 trips, based on the Institute of Transportation Engineer's Trip Generation Manual.

The proposed land use intensities are significantly below the maximum allowed land uses for the entire development site. There is no need to conduct a traffic study at this stage. *Please refer to Sheet A1 Land Use Plan.*

- h. At each phase of the Final PUD, the applicant shall demonstrate that a minimum of 20 percent of the net developable area within the current phase combined with any previously approved phases will be dedicated as common open space to be owned in accordance with FCC 10-23-5-G and maintained in accordance with FCC 10-23-6.

The above condition of approval was based on the assumption that Phase 1 would be the northern portion of the development site including the Open Space Tract A. This Phase 1 Final PUD contains 2.4 acres of open space or about 18.5 percent of the net developable area within the phase boundary. The location and configuration of the open space for this portion of Cannery Station is substantially consistent with the Preliminary PUD. This Phase 1 Final PUD does not hinder or prevent achieving the amount of needed open space. In addition, during Design Review additional small open space areas may be identified depending on the final site layout and building designs.

Additional small open space areas are identified in the Plan. An additional 1.8% of the net developable area within Blocks will be developed as part of the design and construction of each Block. Sheet A1.2 includes a table that lists the size of each open space area. The small open space areas are

identified by the letters "OS" (open space) and the Lot Number. *Please refer to Sheet A1.2 Open Space Plan.*

The Landscape Plan for Lot 1 shows how open space OS-1 is developed with the design for the building. A plaza is created at the intersection of Redwood Street and 47th Street with large planters, a bench, and bike rack. *Please refer to Sheet L-1.*

- i. Before installation of public improvements for the "South 47th" or the "Spruce" Phase of the PUD (as illustrated in Exhibit 45, Sheet A1.3) has begun, the wall between Florentine Estates and the project site shall be constructed.

This Phase 1 Final PUD will include the design and construction of the wall between Florentine Estates and Cannery Station within the phase boundary. A few of the reasons for this approach include:

- Phase 1 includes minimal public improvements within the "Spruce" Phase of the PUD.
- Phase 1 will retain a significant open space buffer between Florentine Estates and Cannery Station LLC.
- Construction of the entire length of wall at this time would cause the premature removal of significant vegetation, including a nice stand of mature trees.
- Construction of the entire length of wall at this time would not be based on the final grading and landscape plans for the entire area adjacent to the wall. Without the area being landscaped, it would likely cause non-native invasive plants and noxious weeds to encroach into the area.
- Construction of the entire length of wall during this phase would not be based on a final grading plan for the townhouses and Open Space Tract D. This could result in the height of the wall not achieving the desired result.
- Construction of the entire length of wall at this time could cause drainage problems. Since the wall would be constructed prior to the final design and construction of stormwater drainage system for the area immediately west of the wall, it could cause drainage issues.

To ensure that construction activity does not encroach upon the existing landscape buffer between Florentine Estates and Cannery Station, prior to any development activity, the developer will clearly mark with orange construction fencing the perimeter of the area of impact.

6. The pavement on Redwood Street shall be designed to handle the size and weight of any proposed transit vehicle expected to be used with consideration to the annual number of trips to be made by the service.

The street construction details will be submitted concurrent with the preliminary subdivision plan and will be subject to City review and approval at that time.

7. A minimum of one covered parking space per single-family home on lots 13 through 32 (per FCC 10-23-5-E) is allowed with the condition that: 1) the driveway is a minimum of 19 feet in depth to allow a second car to park on-site, and 2) the required covered parking space shall not be converted into a living space.

Condition #7 is not applicable since this Final PUD does not include lots 13 thru 32. This Final PUD rennumbers these as lots.

8. If the cottage-style units on the assisted living campus (lot 2) include a garage or carport...

Any garage or carports associated with the cottage-style units on the assisted living campus will be designed to comply with the above condition. Initially, the entrances to the cottages facing Spruce Street may be limited to pedestrian access until both sides of the street are fully improved and the wall is constructed between Florentine Estates and Cannery Station.

9. Prior to application for Phase I Final PUD, the applicant will present to the Community Development Department a signed "Acceptance Agreement" of all conditions of approval.

Condition #9 is met. The applicant signed an "Acceptance Agreement" that is on record at the planning office.

10. Prior to application for Phase I Final PUD, the applicant shall have applied for all necessary removal-fill permits and/or drainage into wetlands from the Department of State Lands and from the US Army Corps of Engineers.

The Joint Permit Application (JPA) for necessary removal fill permits and/or drainage into wetlands, has been applied for. The JPA has been signed by all required jurisdictions and the owner has paid the application fees. The JPA was submitted to US Army Corps of Engineers, Portland District, and Oregon Department of State Lands. A copy of the complete application is listed in the exhibits.

11. Prior to Phase I Final PUD approval, the applicant shall change the project name from Cannery Hill to Munsel Crossing or another more appropriate project name in order to avoid duplication of place names within the City.

Condition #11 is met. The project name is Cannery Station. The name of the property owner was also changed to Cannery Station, LLC.

12. Prior to Phase I Final PUD approval, the applicant shall submit plans for the entire 17-acre site to the City and receive approval from Public Works and Community Development for the following:
 - a. An alternate solution to having a sewer pump station at the north end of project and examine the feasibility of providing gravity sewer service to the entire project site;
 - b. Grading and Erosion Control Plan;
 - c. Vegetation Clearing and Re-vegetation Plan;
 - d. Stormwater Management Plan;

- e. Utility Plan for Sewer within the public way, which includes field verification of the sanitary sewer rim and invert elevations on Highway 101; and
- f. Utility Plan for Water within the public way, which includes an extension of an 8-inch water line southerly along Hwy 101 frontage and terminate at the south property line with standard 2-inch blow-off assembly (if feasible, given site constraints, a fire hydrant is preferred).

These plans shall include a development schedule to address the phasing of the project.

As noted previously, the City of Florence and Cannery Station LLC reached a written agreement on how to provide City services to the subject property. The utility plan submitted with this Final PUD Phase 1 demonstrates compliance with the above condition. *Please refer to Sheet C3.0 Utility Plan.*

- 13. Prior to Phase I Final PUD approval, the CC&Rs will need to be revised to include the following amendments:**
- a. Buildings as defined by FCC 10-1-4 are prohibited within the 35-foot setback from the eastern property boundary next to Florentine Estates;
 - b. The required covered parking shall not be converting to living space; and
 - c. Dedication and maintenance of open space shall be in accordance with FCC 10-23-5-G and FCC 10-23-6.

The draft CC&Rs were revised to incorporate the amendments above and to reflect the new name for the development. Please refer to changes in ARTICLE VII EASEMENTS AND RESTRICTIONS, Sections 7.1 Common Area, 7.2 Maintenance and Repair of Common Areas, 7.4 Building Setback Adjacent to Florentine Estates, and Section 7.5 Covered Parking. *(Refer to Draft Cannery Station CC & R's under separate cover.)*

- 14. As part of Final PUD and Preliminary Subdivision Plan application for any phase abutting or requiring access to Munsel Lake Road, the applicant shall obtain an access permit to Munsel Lake Road from Lane County and provide for additional right-of-way on Munsel Lake Road to meet a minimum of 70-foot wide right-of-way, and building setback a minimum of 30 feet from the right-of-way line, consistent with Lane Code Chapter 15 as shown on Exhibit 49.**

This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable.

- 15. As part of the Final Subdivision Plat application for any phase abutting or requiring access to Munsel Lake Road, the applicant shall dedicate the necessary right-of-way to meet the minimum 70-foot wide right of way for Munsel Lake Road.**

This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable.

- 16. Prior to Final Subdivision Plat Approval for any phase abutting or requiring access to Munsel Lake Road**, the applicant shall have secured a facility permit for the Munsel Lake Road improvements, and the related construction drawings shall be approved by Lane County.

This Phase 1 Final PUD does not abut or require access to Munsel Lake Road. This condition is not applicable.

- 17. Prior to Phase I Final Subdivision Plat Approval**, applicant shall have secured all approach permits deemed necessary by the Oregon Department of Transportation (ODOT).

Condition #17 is not applicable to the Final PUD and will be reviewed during the subdivision process. ODOT has approved approach permits deemed necessary for the full build-out of Cannery Station. For the Phase 1 Final Subdivision Plat, the applicant submitted verification that ODOT has issued the approach permit necessary for 47th Street access at Highway 101.

- 18. Prior to issuance of a Certificate of Occupancy for each development phase**, applicant shall have mitigated associated traffic impacts as determined and approved by ODOT. The design and construction of the proposed improvements to mitigate the traffic impacts must be approved by ODOT and be consistent with the requirements of the approved permits.

Condition #18 is not applicable to the Final PUD and will be reviewed during the building permit process.

- 19. Prior to Final Subdivision Plat Approval for any phase abutting 47th Street (unless ODOT determines it is needed sooner)**, applicant shall construct a new public street connection to Highway 101 (47th Street) approximately 992 feet south of Munsel Lake Road.

Condition #19 is not applicable to the Final PUD and will be reviewed during the subdivision process.

- 20. Prior to issuance of a Certificate of Occupancy for any phase abutting or requiring access to Munsel Lake Road**, all Munsel Lake Road improvements, including pedestrian and landscaping improvements, shall be completed. Applicant shall provide a statement of completion of off-site improvements from Lane County to the City to show that this requirement has been met.

Condition #20 is not applicable to this Phase 1 Final PUD since it does not abut or require access to Munsel Lake Road.

- 21. As part of the Final PUD application for the applicable phase**, the south ends of Spruce and Redwood Streets shall be designed to address vehicular turn-around provisions such as a cul-de-sac or other acceptable standard dead-end treatment.

This Final PUD contains vehicular turn-around areas at the temporary terminus of Spruce and Redwood Streets. These turn-around areas will function for fire and emergency medical response and other motor vehicles. As the streets are further extended, the interim turn-around areas will be shifted.

If only a segment of 47th Street is constructed, the Final PUD application will include a temporary turn-around area for motor vehicles.

- 22. As part of the Final PUD application for the applicable phase, a pedestrian refuge shall be incorporated into the design of the median at the intersection of the 47th Street and Highway 101.**

We are unsure at this time what phase of the Cannery Station project will trigger the need for the median at the intersection of the 47th Street and Highway 101. It is unknown whether ODOT will require the pedestrian refuge designed in the early phase of the project or whether they prefer to wait until the entire Highway 101 improvements are completed.

- 23. Prior to Final Subdivision Plat approval of those areas affected by a wetland, the applicant shall 1) have acquired all required permits, such as removal-fill permit and/or drainage into wetlands from the Department of State Lands (DSL), and from US Army Corps of Engineers (USACOE), and 2) record a deed restriction to prevent current and future impacts to wetland areas with Lane County Deeds and Records and provide a copy to the City.**

Condition #23 is not applicable to the Final PUD and will be reviewed during the subdivision process.

- 24. Prior to Final PUD approval of each phase, the applicant shall meet the following requirements of the Florence Public Works Department:**
- a. Streets shall meet requirements of Public Works in order to be dedicated to the City and for any street proposed to be private, the developer is to provide a means through an owners association, to ensure maintenance of the street and other private infrastructure is provided.
 - b. Construction drawings shall include:
 - i. Profiles of all utilities (sanitary sewer, stormwater and water);
 - ii. All service lateral locations, including water meters, Fire Department Connection (FDC) and Double Detector Check Valve (DDCV) assembly locations;
 - iii. Street light design and locations; and
 - iv. Standards for all utilities (sewer, water, stormwater, street lighting).

All streets will be dedicated to the City.

All streets within Phase 1 are designed according to City street standards and in accordance with the approved Preliminary PUD. Construction drawings will be reviewed during the subdivision and PEPI process.

To contain costs and enable the first building in the project to be economically viable, the developer is requesting that some of the streets be partially completed with final improvements done as the project is built-out.

- 25. Prior to Final PUD approval**, a zoning variance application shall be submitted by the applicant to the Community Development Department if a variance is requested from the parking aisle width of FCC 10-3 Off-Street Parking and Loading standards in effect on March 28, 2008.

No zoning variance is requested at this time.

- 26. As part of the Spruce Phase Preliminary and Final Subdivision applications**, the applicant shall include the 35-foot setback line from the eastern property line on the plan/plat.

Condition #26 is not applicable to this Final PUD.

- 27. As part of the Design Review application**, a lighting plan shall be submitted to the Community Development Department, which includes pedestrian scale, lower intensity, downward directed lighting, with lighting levels measured in foot-candles.

Condition #27 is not applicable to this Final PUD since no on-site lighting is proposed at this time. Street lights will be provided and will be reviewed as part of the street construction drawings at a later date.

- 28. As part of the Design Review application**, building elevations shall be submitted to the Community Development Department and the design and materials will be evaluated for consistency with the coastal village character of Florence.

This Final PUD and Design Review application includes building elevations that are compatible with the coastal character of Florence. Colors and exterior building materials proposed for the building on Lot 1 are included. Original color and material samples are submitted under separate cover. A computer generated color drawing is included in exhibit H. A material and color sample board will be made available for review for accurate color evaluation. *(Please refer to Sheets A2.1, A2.2 A2.3 and A2.4.)*

- 29. Prior to issuance of a building permit**, a soils report and verification of soils bearing capacity and a grading plan, including storm-water design is required. The plan must be prepared by a registered engineer.

Condition #29 is not applicable to this Final PUD and will be reviewed during the building permit process.

- 30. Prior to initiating site development**, the applicant shall submit a permit application to DEQ for a 1200-C construction stormwater permit with Erosion and Sediment Control Plan, and provide a copy of the approved plan and permit to the Community Development Department.

An NPDES General Permit 1200-C Application has been submitted to DEQ. *Please refer to Exhibit I NPDES Permit.*

31. Prior to initiating site development, the applicant shall contact the Confederated Tribes of Coos, Lower Umpqua, and Siuslaw Indians at a least 72 hours notice prior to any ground disturbing activities, so a tribal representative can be present during these activities; and if any known or suspected cultural resources are encountered during any phase of the project, contact the Tribes immediately.

Condition #31 is not applicable to this Final PUD since no site development is proposed at this time.

IV. **ADDITIONAL INFORMATION FOR DESIGN REVIEW, LOT 1**

A. Site Plan for Lot 1

An enlarged Site Plan shows the parking lot layout and dimensions for the building on Lot 1. The plan also shows, setback dimensions, and parking lot dimensions. *Please refer to Sheet C6.3, Lot 1 Street Plan.*

B. Detailed Landscape Plan for Lot 1

The landscape plan shows all proposed plantings, landscape buffers, hardscapes, benches, and other landscape features for the building on Lot 1. A performance specification for irrigation is also included on the plan. *Please refer to Sheet L-1 Landscape Plan.*

C. Floor Plan for Lot 1

An enlarged floor plan shows a proposed medical office building for Lot 1. The building is 4,886 gross square feet. For the purpose of calculating the number of required parking spaces, and using the parking lot standards allowed for this project, 1,387 square feet can be deducted from the total square footage. The result is 3,499 square feet (subtracting private offices, restrooms, and storage areas). Using the one parking space per 200 square feet for medical office, 17.5 spaces are required. 18 spaces are proposed. *Please refer to Sheet A2.4 Lot 1 and Sheet C6.3 Lot 1 Street Plan.*

D. Street Grades

All proposed street grades and slopes shown as a percent grade, are indicated on the Grading Plan. *Please refer to Sheet C2.0 Mass Grading Plan.*

E. Fire Protection

NFPA 13 fire sprinkler systems will be evaluated for each building in the context of the Building Codes in effect at the time of permit application. Buildings proposed as residential use in the Plan would be considered an R Occupancy and would require Automatic Sprinkler Systems per section 903 of the Oregon Structural Specialty

Code. The Development Data chart on the Land Use Plan shows which buildings are proposed for residential use. *Please refer to Sheet A1.1 Land Use Plan, Updated PUD Development Data.*

Fire hydrants are shown, including additional hydrants shown at the entrance to 47th on the south side and one at the apartment entrance on the north side of 47th. *Please refer to Sheet C3.0 Utility Plan.*

Attachments Provided with the May 17, 2012 Compliance Statement

- Exhibit A. Tax Lot Map
- Exhibit B. Legal Description

Documents Printed Separately as part of the May 17, 2012 Compliance Statement

- Phase 1 Final PUD Drawings
- Highway 101 Improvements - Dated 5/8/12 Prepared by JRH Transportation Engineering
- Draft January 4, 2010 Cannery Station, Florence, Oregon Declaration of Covenants, Conditions and Restrictions

New Attachments

Exhibit C.	Aerial Photo	1 pdf file
Exhibit D.	Sewer Map	1 pdf file
Exhibit E.	Storm Water Map	1 pdf file
Exhibit F.	Water Map	1 pdf file
Exhibit G.	Joint Permit Application for Wetlands (JPA)	3 pdf files
Exhibit H.	Lot 1 Exterior Building Materials and Colors	1 pdf file
Exhibit I.	NPDES 1200-C Application	6 pdf files

New Documents Printed Separately

- Phase 1 Final PUD Drawings with Revisions 22 pdf files

TENTATIVE PUD DEVELOPMENT DATA

Lot	Lot Area	Anticipated Commercial SF	Residential Units			Examples of Maximum SF per Use				Parking Required	Parking Provided			
			Senior Hsg	Mid-Rise Apt	Duets & Townhouses	Restaurant*	Medical Office	General Office	Retail					
			1 / 8 brms	1 / 1 bdrm 1.5 / 2 bdrm	1 / unit	1/125	1/200	1/400	1/333					
LOT 1	34,237	3,500 - 6,400	-	-	-	-	-	-	4,000	6,400	12,800	10,560	32	32
LOT 2	83,049	-	75	82	-	-	-	-	-	-	-	-	33**	33
LOT 3	23,096	1,800 - 2,500	-	-	-	-	-	-	1,875	3,000	6,000	4,995	15	15
LOT 4	39,355	6,500 - 8,000	-	-	-	-	-	-	3,375	5,400	10,800	8,991	27	27
LOT 5	31,675	7,450 - 12,350	-	-	-	-	-	-	4,000	6,400	12,800	10,656	32	32
LOT 6	23,132	3,500 - 7,000	-	-	-	-	-	-	2,625	4,200	8,400	6,993	21	21
LOT 7	17,255	4,000 - 4,800	-	-	-	-	-	-	1,875	3,000	6,000	4,995	15	15
LOT 8	19,269	4,500 - 5,250	-	-	-	-	-	-	2,375	3,800	7,600	6,327	19	19
LOT 9	14,979	4,000 - 4,600	-	-	-	-	-	-	2,000	3,200	6,400	5,328	16	16
LOT 10	18,720	4,325 - 5,000	-	-	-	-	-	-	2,250	3,600	7,200	5,994	18	18
LOT 11 All Res	59,538	-	-	-	30	40	-	-	-	-	-	-	32-48	51
LOT 12 All Res	47,735	-	-	-	40	50	-	-	-	-	-	-	42-58	61
LOTS 13-22	28,283	-	-	-	-	-	10	-	-	-	-	-	20	20
LOTS 23-24	6,638	-	-	-	-	-	2	-	-	-	-	-	4	4
LOTS 25-32	20,407	-	-	-	-	-	8	-	-	-	-	-	16	16
TOTAL	467,368	39,575 - 55,900	75 - 82	70 - 90	20 -	-	-	-	-	-	-	-	342-374	380

** 1 parking space / 8 beds + employee parking

Options for Residential and Retail Mixture at LOTS 11 & 12:

Lot	Lot Area	Anticipated Commercial SF	Residential Units			Maximum Square Feet per Use SF				Parking Required	Parking Provided			
			Senior Hsg	Mid-Rise Apt	Duets & Townhouses	Restaurant	Medical Office	General Office	Retail					
			1 / 8 brms	1 / 1 bdrm 1.5 / 2 bdrm	1 / unit	1/125	1/200	1/400	1/333					
LOT 11 All 1		2,000 - 12,000	-	-	20 - 24	-	-	-	-	-	-	12,000	58-66	51
LOT 11 All 2		2,000 - 12,000	-	-	20 - 24	-	-	-	2,000	-	-	6,000	52-60	51
LOT 12 All 1		2,000 - 8,000	-	-	30 - 34	-	-	-	-	-	-	8,000	56-64	61
LOT 12 All 2		2,000 - 8,000	-	-	32 - 34	-	-	-	2,000	-	-	6,000	66-74	61

UPDATED PUD DEVELOPMENT DATA

Tentative PUD	Final PUD	Lot Area	Anticipated Commercial SF	Residential Units			Examples of Maximum SF per Use				Parking Required	Parking Provided		
				Senior Hsg	Mid-Rise Apt	Duets & Townhouses	Restaurant*	Medical Office	General Office	Retail				
				1 / 8 brms	1 / 1 bdrm 1.5 / 2 bdrm	1 / unit	1 / 125	1 / 200	1 / 400	1 / 333				
LOT 10	LOT 1	18,690	4,325 - 5,000	-	-	-	-	-	2,250	3,600	7,200	5,994	18	18
LOT 2	LOT 2	83,049	-	75	82	-	-	-	-	-	-	-	33**	33
LOTS 23-24	LOT 3-4	6,638	-	-	-	-	-	-	2	-	-	-	4	4
LOT 12 All Res	LOT 5 All Res	47,735	-	-	40	50	-	-	-	-	-	-	42-58	61
LOT 9	LOT 6	14,979	4,000 - 4,600	-	-	-	-	-	2,000	3,200	6,400	5,328	16	17
LOT 8	LOT 7	19,270	4,500 - 5,250	-	-	-	-	-	2,375	3,800	7,600	6,327	19	16
LOT 7	LOT 8	17,289	4,000 - 4,800	-	-	-	-	-	1,875	3,000	6,000	4,995	15	17
LOT 6	LOT 9	23,126	3,500 - 7,000	-	-	-	-	-	2,625	4,200	8,400	6,993	21	21
LOT 5	LOT 10	31,610	7,450 - 12,350	-	-	-	-	-	4,000	6,400	12,800	10,656	32	45
LOT 4	LOT 11	39,578	6,500 - 8,000	-	-	-	-	-	3,375	5,400	10,800	8,991	27	17
LOT 3	LOT 12	23,705	1,800 - 2,500	-	-	-	-	-	1,875	3,000	6,000	4,995	15	15
LOT 1	LOT 13	35,070	3,500 - 6,400	-	-	-	-	-	4,000	6,400	12,800	10,560	32	33
LOTS 13-22	LOTS 14-23	29,267	-	-	-	-	10	-	-	-	-	-	20	20
LOTS 25-32	LOTS 24-31	20,409	-	-	-	-	8	-	-	-	-	-	16	16
LOT 11 All Res	LOT 32 All Res	59,495	-	-	30	40	-	-	-	-	-	-	32-48	51
TOTAL		469,888	39,575 - 55,900	75 - 82	70 - 90	-	-	-	-	-	-	-	342-374	384

** 1 parking space / 8 beds + employee parking

Options for Residential and Retail Mixture at LOTS 11 & 12:

Tentative PUD	Final PUD	Lot Area	Anticipated Commercial SF	Residential Units			Examples of Maximum SF per Use				Parking Required	Parking Provided	
				Senior Hsg	Mid-Rise Apt	Duets & Townhouses	Restaurant*	Medical Office	General Office	Retail			
				1 / 8 brms	1 / 1 bdrm 1.5 / 2 bdrm	1 / unit	1 / 125	1 / 200	1 / 400	1 / 333			
LOT 11 All 1	LOT 32 All 1	2,000	- 12,000	-	-	20 - 24	-	-	-	7,200	12,000	58-66	51
LOT 11 All 2	LOT 32 All 2	2,000	- 12,000	-	-	20 - 24	-	-	2,000	-	6,000	52-60	51
LOT 12 All 1	LOT 5 All 1	2,000	- 8,000	-	-	30 - 34	-	-	-	-	8,000	56-64	61
LOT 11 All 2	LOT 5 All 2	2,000	- 8,000	-	-	30 - 34	-	-	2,000	-	6,000	66-74	61

CANNERY STATION - FLORENCE, OREGON

**FINAL PUD: PHASE 1
LOTS 1 - 6**

06/14/2013

OWNER

CANNERY STATION, LLC
2911 TENNYSON AVE, SU 400
EUGENE, OR 97408
TEL: (541) 344-5500
FAX: (541) 485-2550

CONTACT(S):
TERESA BISHOW, AICP

DESIGN TEAM

PLANNER
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EUGENE, OR 97408
TEL: (541) 344-5500
FAX: (541) 485-2550

CONTACT(S):
TERESA BISHOW, AICP

ARCHITECT
TONY KOACH ARCHITECT
2301 NW THURMAN
SUITE K
PORTLAND, OREGON 97210
TEL: (503) 358-4602

CONTACT(S):
TONY KOACH

LANDSCAPE ARCHITECT
DOUGHERTY LANDSCAPE ARCHITECTS, INC.
474 WILLAMETTE STREET, SUITE 305
EUGENE, OREGON 97401
TEL: (541) 683-5803
FAX: (541) 683-8183

CONTACT(S):
DAVID DOUGHERTY, ASLA

CIVIL ENGINEER
KPF CONSULTING ENGINEERS
1201 OAK STREET, SUITE 100
EUGENE, OR 97401
TEL: (541) 684-4902
FAX: (541) 684-4909

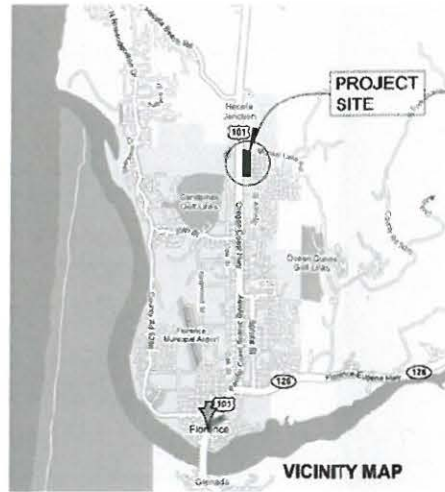
CONTACT(S):
MATT KEENAN, PE
JENNIFER HOLCOMB, PE

TRAFFIC ENGINEER
JRH TRANSPORTATION ENGINEERING
4765 VILLAGE PLAZA LOOP, SUITE 201
EUGENE, OREGON 97401
TEL: (541) 687-1081
FAX: (541) 345-6599

CONTACT(S):
Kelly Sandow, PE

SURVEYOR
WOBBE & ASSOCIATES, INC.
510 KINGWOOD STREET
FLORENCE, OREGON 97439
TEL: (503) 771-1988
FAX: (503) 772-2496

CONTACT(S):
EUGENE WOBBE, PLS



DRAWING INDEX

ARCHITECTURAL	CIVIL ENGINEERING	LANDSCAPE ARCHITECTURE
A1.1 LAND USE PLAN	C1.0 EXISTING CONDITIONS PLAN	LA-1 Existing Vegetation Plan
A1.2 OPEN SPACE PLAN	C2.0 MASS GRADING PLAN	LA-2 Landscape Master Plan
A1.3 PARKING PLAN	C2.1 LOT 1 MASS GRADING PLAN	LA-3 Plant Lists and Details
A2.1 LOTS 1 & 6 - SITE PLAN & ELEVATIONS	C3.0 UTILITY PLAN	L-1 Lot 1 Landscape Plan
A2.2 LOT 2 - SITE PLAN & ELEVATIONS	C3.1 LOT 1 UTILITY PLAN	
A2.3 LOTS 3-5 - SITE PLAN & ELEVATIONS	C6.0 STREET PLAN	
A2.4 LOT 1 - PLAN & ELEVATIONS	C6.1 STREET TYPICAL SECTIONS	
	C6.2 STREET INTERSECTION DETAILS/SECTIONS	
	C6.3 LOT 1 STREET PLAN	
	EC1.0 EROSION CONTROL COVER SHEET	
	EC2.0 EROSION CONTROL PLAN	
	EC3.0 EROSION CONTROL DETAILS	

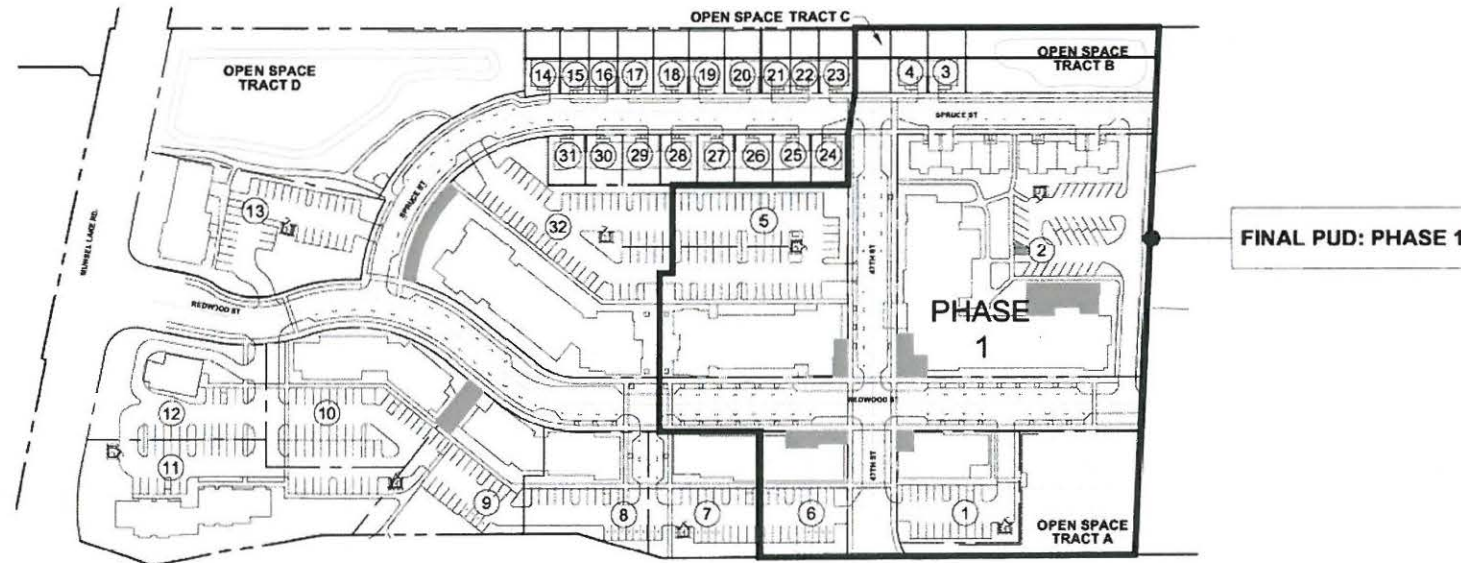


Exhibit 15

APPROVED
City of Florence
Community Development
Department
15
Exhibit
PC1212 FPUD01
File Number

1 LAND USE PLAN
A1.1 Scale: 1"=100'-0"



LEGEND

- PUD BOUNDARY
- - - PROPERTY LINE
- ① LOT #
- TRACT OPEN SPACE

ASSESSOR MAP 18-12-14-20
TAX LOT 700
87344 MUNSEL LAKE ROAD

TONY KOACH
ARCHITECT
2301 N.W. Thurman
Suite K
Portland, Oregon 97210

**CANNERY STATION
FLORENCE, OR**

**FINAL PUD - PHASE 1
LOTS 1 - 6**

CLIENT:



ARLIE & COMPANY
2911 TENNYSON AVENUE
SUITE 400
EUGENE, OREGON 97408
(541) 344-5500
www.arlie.com

ISSUED:

- 10/14/2008 - TENTATIVE PUD
- 05/17/2012 - FINAL PUD - PHASE 1
- 03/18/2013 - REVISION
- 06/19/2013 - REVISION

STAMP:



PROJECT #: 723
DRAWN BY:
CHECKED BY:
FILE NAME:
DATE: 06/14/2013

CANNERY STATION

LAND USE PLAN

A1.1

TONY KOACH
ARCHITECT
2301 N.W. Thurman
Suite K
Portland, Oregon 97210

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
LOTS 1 - 8

CLIENT:

arlie&company

ARLIE & COMPANY
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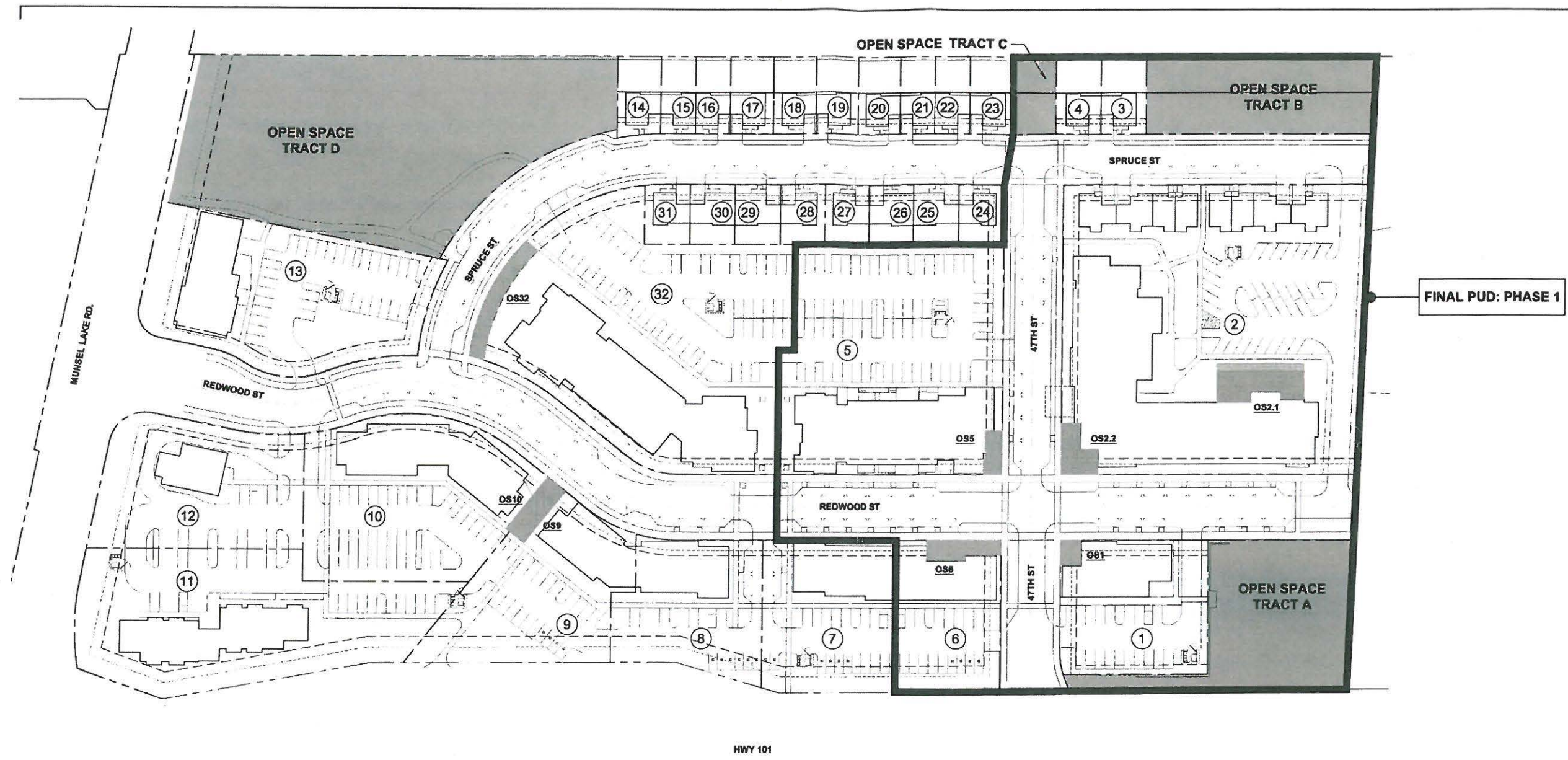
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10/4/2006 - TENTATIVE PUD
05/17/2012 - FINAL PUD - PHASE 1
03/18/2013 - REVISION

STAMP:


PROJECT #: 723
DRAWN BY:
CHECKED BY:
FILE NAME:
DATE: 03/18/2013

CANNERY STATION
DRAWING:
OPEN SPACE PLAN

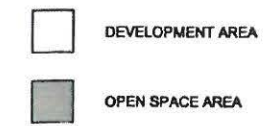
A1.2



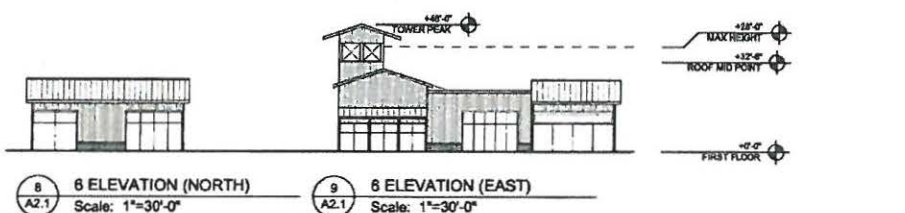
OPEN SPACE		
TRACT A	22,385 SF	STORMWATER DETENTION / RETENTION
TRACT B	16,905 SF	STORMWATER DETENTION / RETENTION, LANDSCAPE BUFFER, POCKET PARK
TRACT C	3,167 SF	LANDSCAPE BUFFER, POCKET PARK
TRACT D	64,050 SF	STORMWATER DETENTION / RETENTION, PARK & RHODODENDRON GARDEN, PRESERVATION OF EXISTING VEGETATION, PEDESTRIAN PATH
TOTAL	106,507 SF	

OPEN SPACE WITHIN BLOCKS	
OS1	500 SF
OS2.1	3,000 SF
OS2.2	1,350 SF
OS5	810 SF
OS6	840 SF
OS9	1,300 SF
OS10	325 SF
OS32	2,000 SF
TOTAL	10,125 SF

APPROVED
City of Florence
Community Development
Department
16
Exhibit
PC1212 FPUD01
File Number

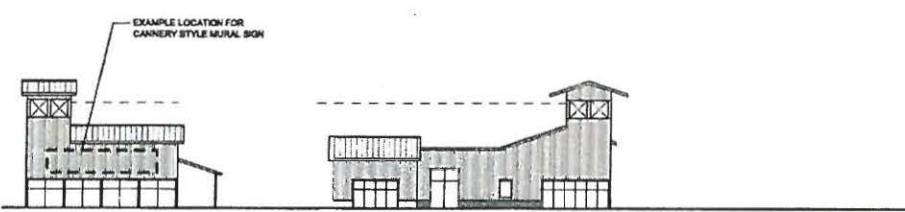


736,329 SF OVERALL DEVELOPMENT AREA
574,077 SF NET DEVELOPMENT AREA EXCLUDING PUBLIC ROADS
86,112 SF OPEN SPACE AREA REQUIRED BY NORTH COMMERCIAL DISTRICT - 15 %
114,815 SF OPEN SPACE AREA REQUIRED BY PUD - 20 %
106,507 SF AREA PROVIDED BY DEDICATED OPEN SPACE TRACTS - 18.4 %
10,125 SF OPEN SPACE AREA REQUIRED WITHIN BLOCKS - 1.8 %
(TO BE DETERMINED DURING FINAL PUD'S)
OPEN SPACE DATA TO BE VERIFIED PENDING DEDICATION FO RIGHT-OF-WAY



6 ELEVATION (NORTH)
A2.1 Scale: 1"=30'-0"

9 ELEVATION (EAST)
A2.1 Scale: 1"=30'-0"



6 ELEVATION (SOUTH)
A2.1 Scale: 1"=30'-0"

7 ELEVATION (WEST)
A2.1 Scale: 1"=30'-0"

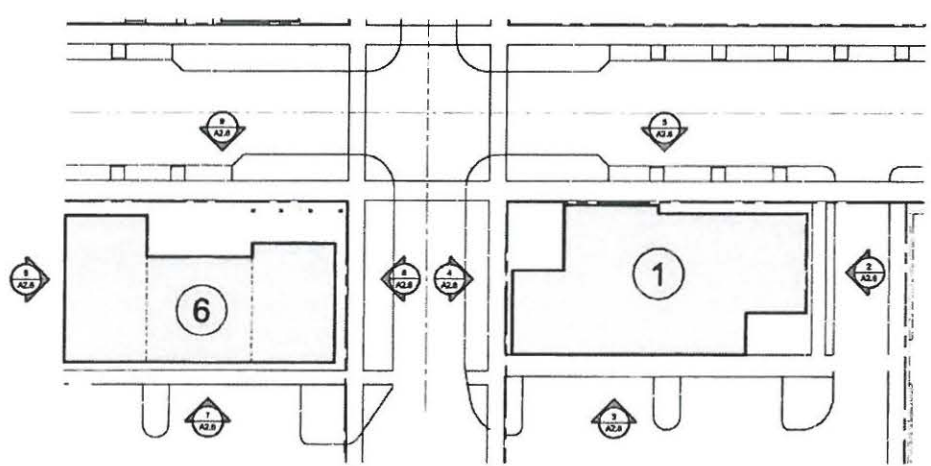


2 ELEVATION (SOUTH)
A2.1 Scale: 1"=30'-0"

3 ELEVATION (WEST)
A2.1 Scale: 1"=30'-0"

4 ELEVATION (NORTH)
A2.1 Scale: 1"=30'-0"

5 ELEVATION (EAST)
A2.1 Scale: 1"=30'-0"



1 REFERENCE PLAN LOTS 6-10
A2.1 Scale: 1"=30'-0"



LOTS 1 & 6:
COMMERCIAL OCCUPANCY, TYP.

PUD CONCEPT DESIGN:

THE BUILDING PLAN AND ELEVATION DRAWINGS IN THIS PUD SUBMITTAL ILLUSTRATE GENERAL DESIGN AND CHARACTER OF THE BUILDINGS. THESE DRAWINGS ARE CONCEPTUAL AND REPRESENT EXAMPLES OF DESIGN ELEMENTS OUTLINED IN THE GUIDELINES BELOW. BUILDING DESIGNS ARE REPRESENTATIONAL AND NOT INTENDED TO BE THE FINAL LAYOUT AND DESIGN.

ARCHITECTURAL DESIGN GUIDELINES

- BUILDING LOCATION:** BUILDINGS ABUTTING ROCKFISH STREET ARE LOCATED CLOSE TO THE STREET TO CREATE A POSITIVE AND LIVELY STREET EDGE. WHERE BUILDINGS STEP BACK FROM ROCKFISH, THEY CREATE POSITIVE OPEN SPACES.
- ENTRIES:** PRIMARY BUILDING ENTRANCES ARE ORIENTED TOWARDS THE STREET TO ENHANCE PEDESTRIAN ACTIVITY.
- BUILDING SCALE & MASSING:** SETBACKS, BAY WIDTHS, BALCONIES AND ROOF FORMS PROVIDE VARIETY AND BREAKUP MASSING AND SCALE.
- BUILDING CORNERS:** TOWERS & TALLER ROOF FORMS MARK SPECIAL CORNERS OR PARTS OF BUILDINGS. BUILDINGS CUT BACK AT CORNERS PROVIDE POSITIVE OPEN SPACE.
- COMMERCIAL BUILDING CHARACTER:** BUILDING CHARACTER INSPIRED BY COASTAL CANNERY ARCHITECTURE, PARTICULARLY MATERIALS AND GABLED AND SHED ROOF FORMS. BLANK AREAS OF WALL MAY BE USED AS LOCATIONS FOR MURAL SIGNAGE.
- RESIDENTIAL BUILDING CHARACTER:** BUILDINGS ALONG SPRUCE STREET ARE SMALLER IN SCALE AND INCORPORATE COASTAL ELEMENTS. VARIED ROOF FORMS EMPHASIZE INDIVIDUAL UNITS. FRONT PORCHES FOSTER COMMUNITY CONNECTIONS.
- GROUND FLOOR COMMERCIAL WINDOWS:** LARGE STOREFRONT WINDOWS ENGAGE LIFE ON THE STREET. RIGID CANOPY, AWNINGS OR SUN SHADES PROVIDE COVER AND ARTICULATE FAÇADES.
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PROJECT:
**CANNERY STATION
FLORENCE, OR**

FINAL PUD - PHASE 1
LOTS 1 - 6



ARLIE & COMPANY
2011 TENNYSON AVENUE
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ISSUED:
10/14/2008 - TENTATIVE PUD
09/17/2012 - FINAL PUD - PHASE 1
09/18/2013 - REVISION



PROJECT #: 720
DRAWN BY:
CHECKED BY:
FILE NAME:
DATE: 09/18/2013

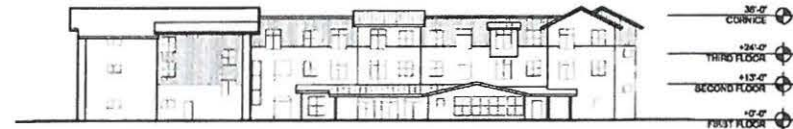
CANNERY STATION
DRAWING:
**LOTS
1 & 6**

A2.1

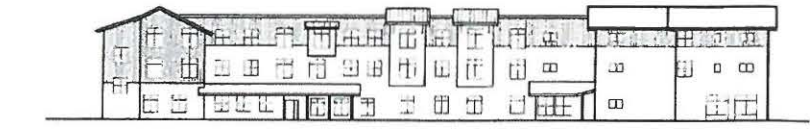
APPROVED
City of Florence
Community Development
Department
17 Exhibit
PC1212 FPUD01
File Number



2 ELEVATION: WEST
 A2.2 Scale: 1"=30'-0"



3 ELEVATION: SOUTH
 A2.2 Scale: 1"=30'-0"



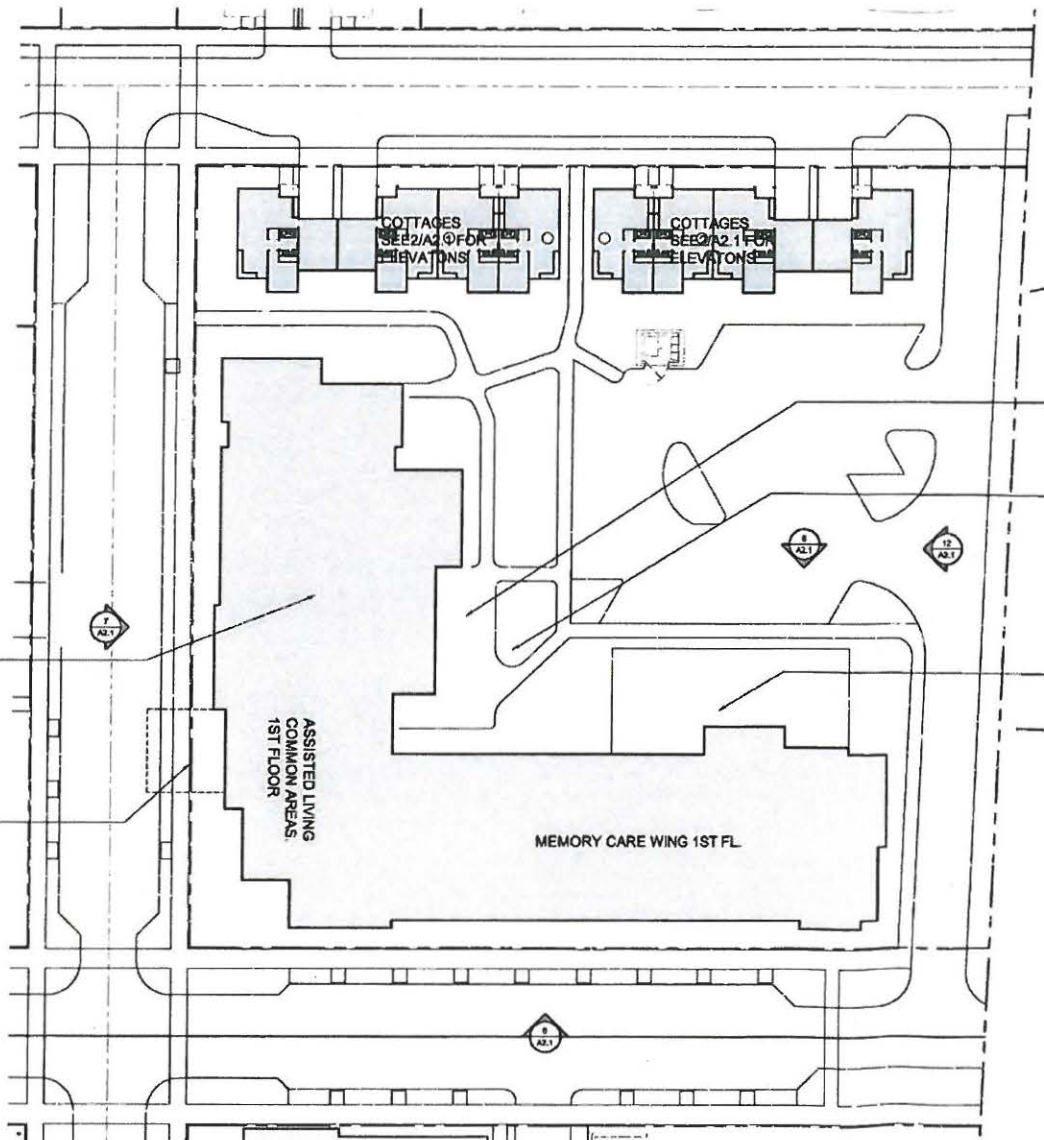
4 ELEVATION: EAST
 A2.2 Scale: 1"=30'-0"



5 ELEVATION: NORTH
 A2.2 Scale: 1"=30'-0"



2 ELEVATION: COTTAGES
 A2.2 Scale: 1"=30'-0"



- LOT 2:**
1. GROUND FLOOR TO BE COMMERCIAL, RESIDENTIAL, ALZHEIMER'S CARE OR COMMUNITY COMMON SPACE.
 2. SECOND AND THIRD FLOOR TO BE RESIDENTIAL (RETIREMENT / ASSISTED LIVING/ INDEPENDENT LIVING)
 3. SOUTH FACING COURTYARD ON INTERIOR OF BLOCK.
 4. COVERED DROP OFF AT NORTH BUILDING ENTRY.

PUD CONCEPT DESIGN:
 THE BUILDING PLAN AND ELEVATION DRAWINGS IN THIS PUD SUBMITTAL ILLUSTRATE GENERAL DESIGN AND CHARACTER OF THE BUILDINGS. THESE DRAWINGS ARE CONCEPTUAL AND REPRESENT EXAMPLES OF DESIGN ELEMENTS OUTLINED IN THE GUIDELINES BELOW. BUILDING DESIGNS ARE REPRESENTATIONAL AND NOT INTENDED TO BE THE FINAL LAYOUT AND DESIGN.

- ARCHITECTURAL DESIGN GUIDELINES**
1. **BUILDING LOCATION:** BUILDINGS ABUTTING ROCKFISH STREET ARE LOCATED CLOSE TO THE STREET TO CREATE A POSITIVE AND LIVELY STREET EDGE. WHERE BUILDINGS STEP BACK FROM ROCKFISH, THEY CREATE POSITIVE OPEN SPACES.
 2. **ENTRIES:** PRIMARY BUILDING ENTRANCES ARE ORIENTED TOWARDS THE STREET TO ENHANCE PEDESTRIAN ACTIVITY.
 3. **BUILDING SCALE & MASSING:** SETBACKS, BAY WIDTHS, BALCONIES AND ROOF FORMS PROVIDE VARIETY AND BREAKUP MASSING AND SCALE.
 4. **BUILDING CORNERS:** TOWERS & TALLER ROOF FORMS MARK SPECIAL CORNERS OR PARTS OF BUILDINGS. BUILDINGS CUT BACK AT CORNERS PROVIDE POSITIVE OPEN SPACE.
 5. **COMMERCIAL BUILDING CHARACTER:** BUILDING CHARACTER INSPIRED BY COASTAL CANNERY ARCHITECTURE, PARTICULARLY MATERIALS AND GABLED AND SHED ROOF FORMS. BLANK AREAS OF WALL MAY BE USED AS LOCATIONS FOR MURAL SIGNAGE.
 6. **RESIDENTIAL BUILDING CHARACTER:** BUILDINGS ALONG SPRUCE STREET ARE SMALLER IN SCALE AND INCORPORATE COASTAL ELEMENTS. VARIED ROOF FORMS EMPHASIZE INDIVIDUAL UNITS. FRONT PORCHES FOSTER COMMUNITY CONNECTIONS
 7. **GROUND FLOOR COMMERCIAL WINDOWS:** LARGE STOREFRONT WINDOWS ENGAGE LIFE ON THE STREET. RIGID CANOPY, AWNINGS OR SUN SHADES PROVIDE COVER AND ARTICULATE FACADES.
 8. **ABOVE GROUND LEVEL WINDOWS:** RESIDENTIAL AND OFFICE WINDOWS CREATE TRADITIONAL WINDOW PATTERNS (NOT HORIZONTAL BANDS). SIZES AND RHYTHMS VARY TO PROVIDE VISUAL INTEREST.
 9. **EXTERIOR BUILDING MATERIALS:** EXAMPLES OF SIDING MATERIALS: BOARD & BATTEN, CEDAR SHINGLES, METAL SIDING, MASONRY, STUCCO, AND CONCRETE ACCENTS. EXAMPLES OF ROOFING MATERIALS: METAL, COMPOSITION SHINGLE, SINGLE-PLY ON LOW SLOPE ROOFS NOT VISIBLE FROM STREET.
 10. **COLORS:** COMMERCIAL BUILDINGS SHALL TEND TOWARD THE MONOCHROMATIC TO EMPHASIZE THE BUILDING FORM AND SHAPE. ACCENT COLORS MAY VARY, SOME BEING SATURATED IN SELECT PLACES. RESIDENTIAL BUILDINGS SHALL TEND TOWARD NATURAL TONES WITH DETAIL FEATURES OF MORE SATURATED COLORS.

THREE STORY RESIDENTIAL: ASSISTED LIVING COMMON SPACE ON FIRST FLOOR. APARTMENTS ON UPPER FLOORS
 COVERED DROP OFF AND MAIN BUILDING ENTRY

1 PLAN - LOT 2 - 1ST FLOOR
 A2.2 Scale: 1"=30'-0"

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 Community Development
 Department
 18 Exhibit
 PC1212FPUD01
 File Number



5 ELEVATION (WEST)
Scale: 1"=30'-0"



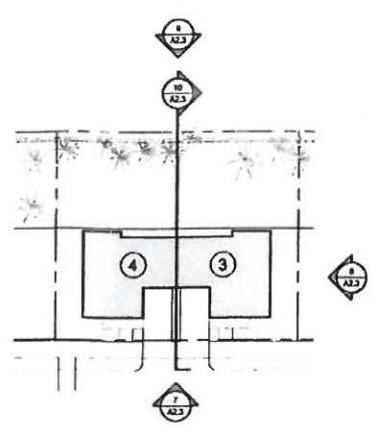
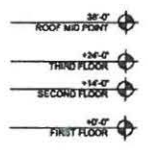
4 ELEVATION (EAST)
Scale: 1"=30'-0"



2 ELEVATION (NORTH)
Scale: 1"=30'-0"



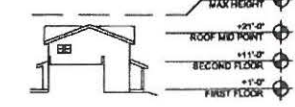
3 ELEVATION (SOUTH)
Scale: 1"=30'-0"



6 SITE PLAN - LOTS 3 & 4
Scale: 1"=30'-0"



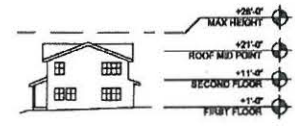
9 ROWHOUSE: ELEVATION
Scale: 1"=30'-0"



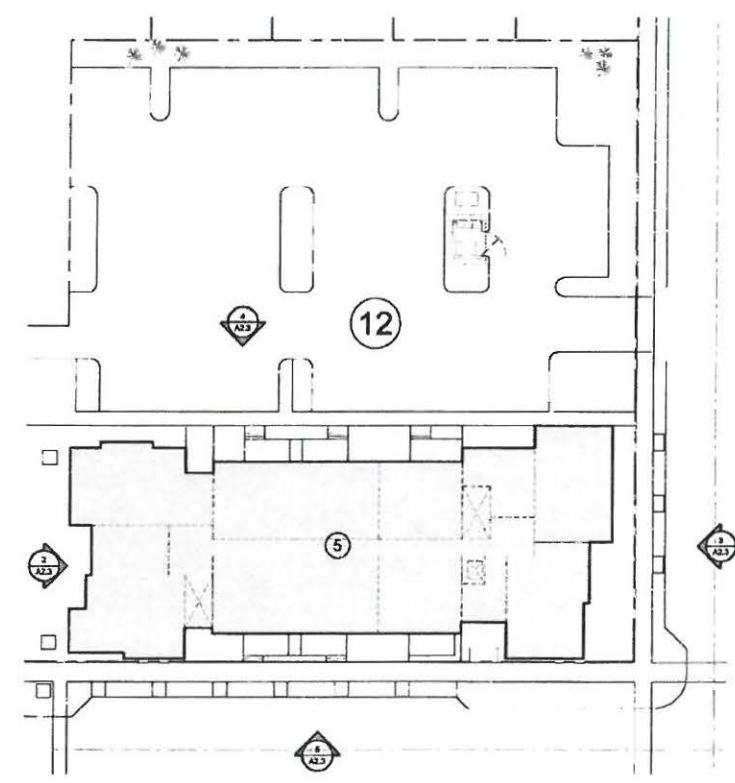
10 ROWHOUSE: ELEVATION
Scale: 1"=30'-0"



7 ROWHOUSE: ELEVATION
Scale: 1"=30'-0"



8 ROWHOUSE: ELEVATION
Scale: 1"=30'-0"



1 SITE PLAN - LOT 5
Scale: 1"=30'-0"



LOTS 11 & 12:
COMMERCIAL & RESIDENTIAL OCCUPANCY.

PUD CONCEPT DESIGN:

THE BUILDING PLAN AND ELEVATION DRAWINGS IN THIS PUD SUBMITTAL ILLUSTRATE GENERAL DESIGN AND CHARACTER OF THE BUILDINGS. THESE DRAWINGS ARE CONCEPTUAL AND REPRESENT EXAMPLES OF DESIGN ELEMENTS OUTLINED IN THE GUIDELINES BELOW. BUILDING DESIGNS ARE REPRESENTATIONAL AND NOT INTENDED TO BE THE FINAL LAYOUT AND DESIGN.

ARCHITECTURAL DESIGN GUIDELINES

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APPROVED
City of Florence
Community Development
Department
19 Exhibit
PC1212FPUD01
File Number

TONY KOACH
ARCHITECT
2301 N.W. Thurman
Suite K
Portland, Oregon 97210

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
LOTS 1 - 6



ARLIE & COMPANY
2611 TENNYSON AVENUE
SUITE 400
EUGENE, OREGON 97408
(541) 344-5600
www.arlie.com

ISSUES:
10/14/2008 - TENTATIVE PUD
05/17/2012 - FINAL PUD - PHASE 1
02/16/2013 - REVISION



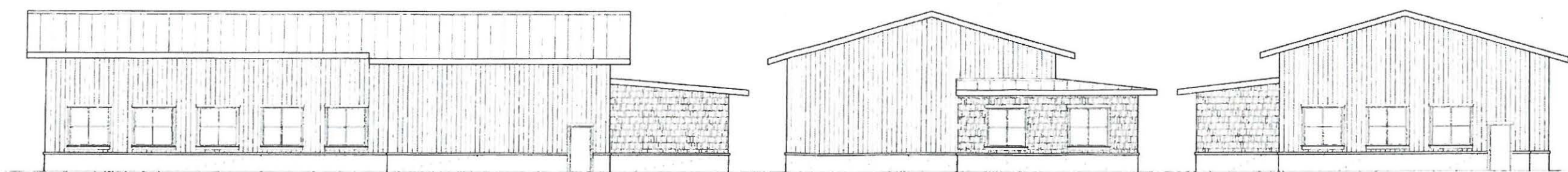
PROJECT #: 722
DRAWN BY:
CHECKED BY:
FILE NAME:
DATE: 02/16/2013

CANNERY STATION

DRAWING:
LOTS 3 - 5

A2.3

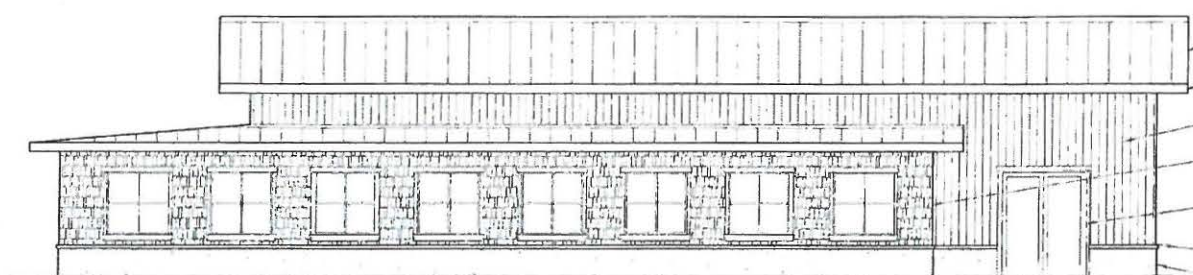
TONY KOACH
 ARCHITECT
 2301 N.W. Thurman
 Suite K
 Portland, Oregon 97210



3 EAST ELEVATION
 A2.4 Scale: 1/8"=1'-0"

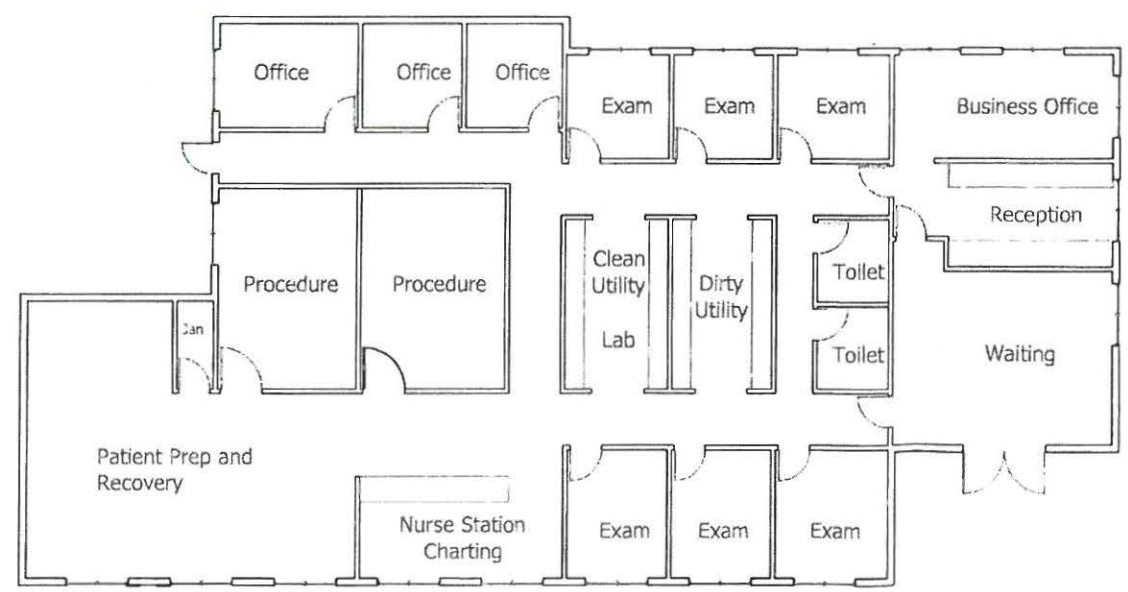
2 NORTH ELEVATION
 A2.4 Scale: 1/8"=1'-0"

1 SOUTH ELEVATION
 A2.4 Scale: 1/8"=1'-0"



4 WEST ELEVATION
 A2.4 Scale: 1/8"=1'-0"

- Standing seam metal roof
- Painted wood fascia
- Painted board and batten siding
- Painted cedar shingles
- Painted wood door and window trim. Vinyl windows and natural anodized aluminum storefront entry doors and frames.
- Painted wood drip ledge
- Sacked, natural finish architectural concrete



5 SCHEMATIC PLAN FOR A MEDICAL OFFICE
 A2.4 Scale: 1/8"=1'-0"

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 City of Florence
 Community Development
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 PC1212 FPUD01
 File Number

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6



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 2511 TENNYSON AVENUE
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 (541) 344-5500
 www.arlie.com

ISSUED:
 10/14/2008 - TENTATIVE PUD
 05/12/2012 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION



PROJECT #: 723
 DRAWN BY:
 CHECKED BY:
 FILE NAME:
 DATE: 03/18/2013

CANNERY STATION

DRAWING:

LOT 1

A2.4

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 2301 N.W. Thurman
 Suite K
 Portland, Oregon 97210



PROJECT:
**CANNERY STATION
 FLORENCE, OR**

FINAL PUD - PHASE 1
 LOTS 1 - 6



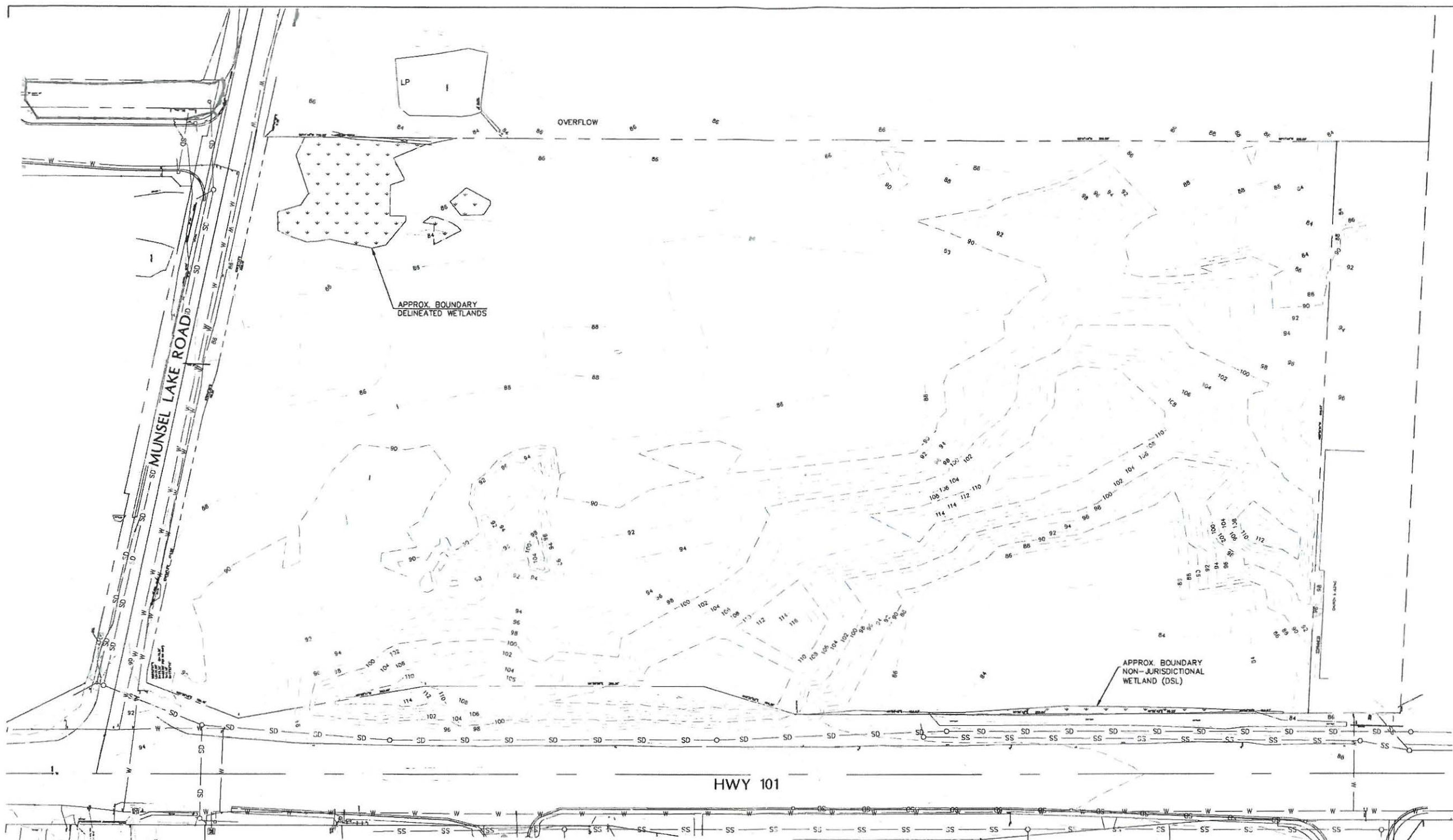
ARFE & COMPANY
 2911 TENNYSON AVENUE
 SUITE 400
 EUGENE, OREGON 97408
 (541) 344-5500
 www.arfe.com

REVISIONS:
 10/14/2008 - TENTATIVE PUD
 09/11/2011 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION



PROJECT # 30327-00
 DESIGNED BY: JMF
 CHECKED BY: MKK
 FILE NAME:
 DATE: 02-18-2013

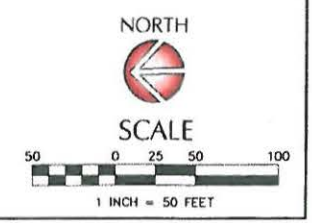
CANNERY STATION
 DRAWING:
C1.0
 EXISTING
 CONDITIONS
 PLAN



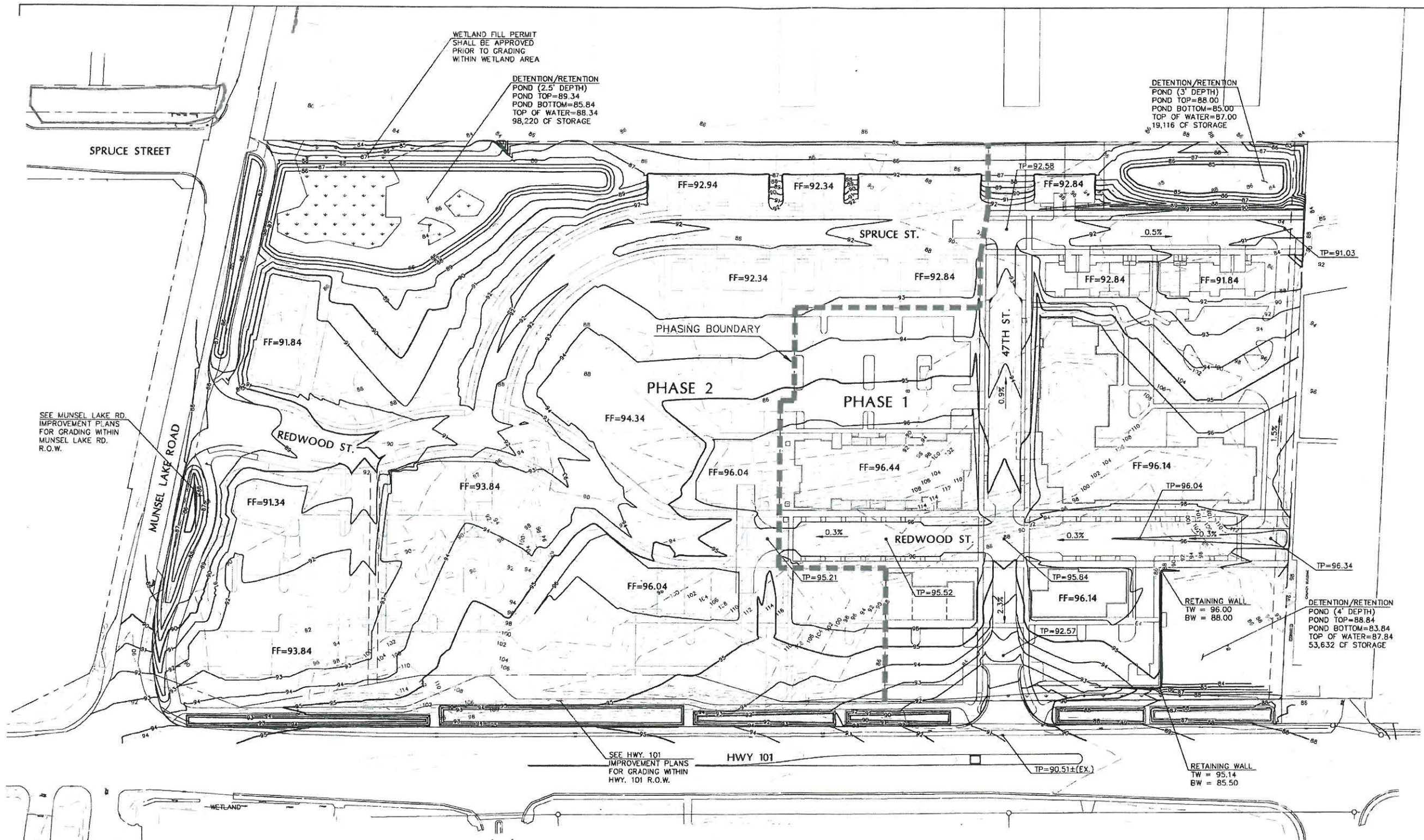
EXISTING	PROPOSED
	FIRE HYDRANT
	WATER VALVE
	POWER POLE
	LIGHT POLE
	PHONE RISER
	ELECTRICAL BOX
	WATER METER
	TRAFFIC SIGN
	CATCH BASIN
	VAULT
	SEWER CLEANOUT
	SEWER MANHOLE
	APPROX. BOUNDARY OF WETLANDS
	PROPERTY LINE
	MINOR CONTOUR
	MAJOR CONTOUR

ABBREVIATIONS	
APPROX.	APPROXIMATE
DSL	DEPARTMENT OF STATE LANDS
HWY	HIGHWAY
ST.	STREET

NOTES:
 1. EXISTING CONDITIONS ARE AS OF MID-MARCH 2008. UTILITY WORK HAS BEEN DONE ALONG HWY 101 ADJACENT TO THE SITE. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING SITE CONDITIONS PRIOR TO SITE INHERITANCE.



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 PC1212FPUD01
 File Number



WETLAND FILL PERMIT SHALL BE APPROVED PRIOR TO GRADING WITHIN WETLAND AREA

DETENTION/RETENTION POND (2.5' DEPTH)
 POND TOP=89.34
 POND BOTTOM=85.84
 TOP OF WATER=88.34
 98,220 CF STORAGE

DETENTION/RETENTION POND (3' DEPTH)
 POND TOP=88.00
 POND BOTTOM=85.00
 TOP OF WATER=87.00
 19,116 CF STORAGE

SEE MUNSEL LAKE RD IMPROVEMENT PLANS FOR GRADING WITHIN MUNSEL LAKE RD. R.O.W.

SEE HWY. 101 IMPROVEMENT PLANS FOR GRADING WITHIN HWY. 101 R.O.W.

RETAINING WALL
 TW = 96.04
 BW = 88.00

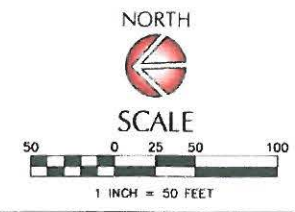
DETENTION/RETENTION POND (4' DEPTH)
 POND TOP=88.84
 POND BOTTOM=83.84
 TOP OF WATER=87.84
 53,632 CF STORAGE

RETAINING WALL
 TW = 95.14
 BW = 85.50

EXISTING	PROPERTY LINE/ RIGHT-OF-WAY	PROPOSED
- - - - -	WETLAND	- - - - -
92	MINOR CONTOUR	92
90	MAJOR CONTOUR	90
- - - - -	TOP/BOTTOM OF POND	- - - - -
- - - - -	PHASING LINE	- - - - -

ABBREVIATIONS	
BW	BOTTOM OF WALL
CF	CUBIC FEET
FF	FINISH FLOOR
HWY	HIGHWAY
HP	HIGH POINT
LP	LOW POINT
ST.	STREET
TP	TOP OF PAVEMENT
R.O.W.	RIGHT-OF-WAY
EXIST.	EXISTING
RD.	ROAD
TW	TOP OF WALL

APPROVED
 City of Florence
 Community Development
 Department
 22
 Exhibit
 PC1212 FPUDD1
 File Number



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 FAX (541) 684-6802

PROJECT
**CANNERY STATION
 FLORENCE, OR**

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT

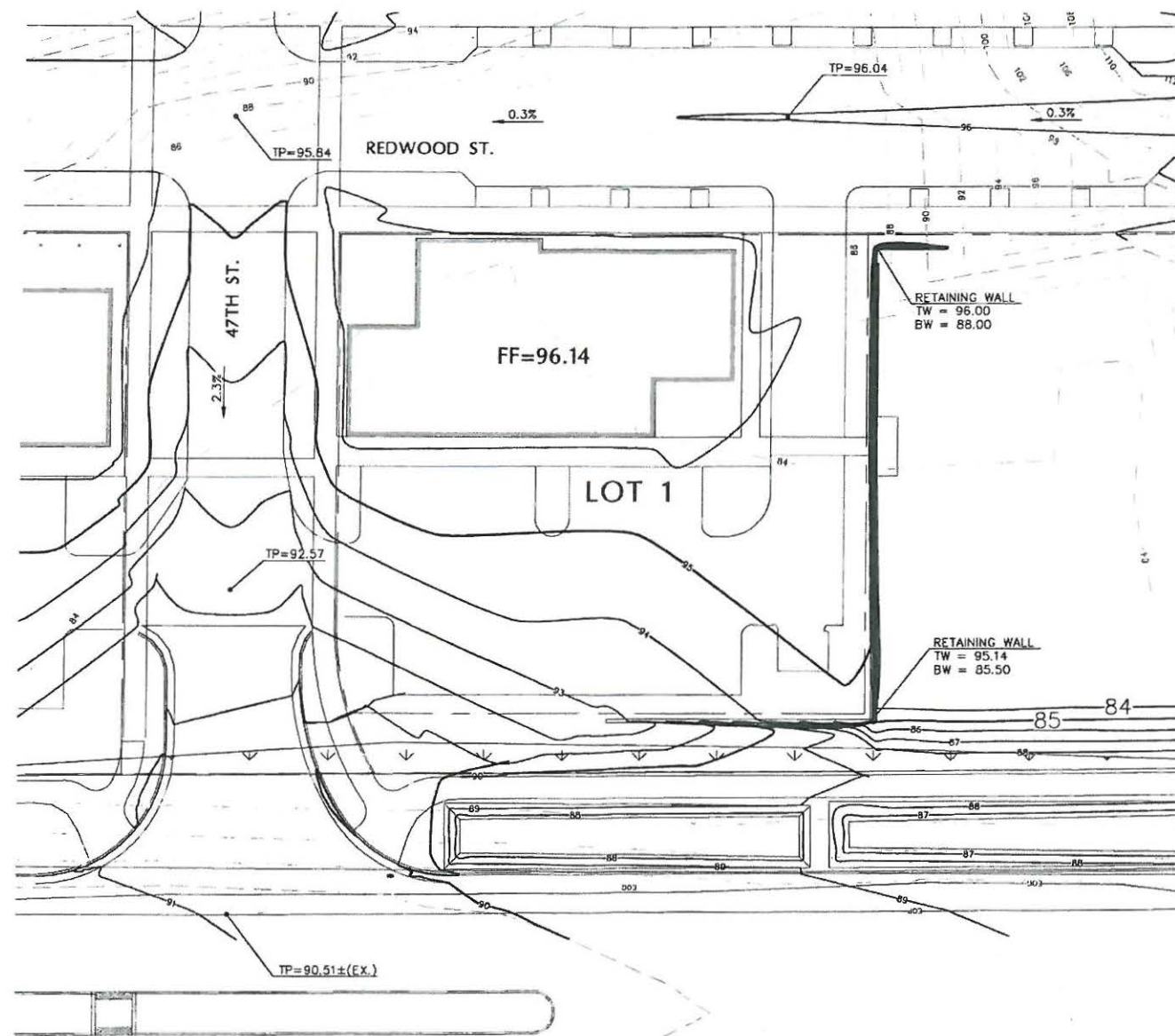
ARLIE & COMPANY
 2911 TENNYSON AVENUE
 SUITE 400
 EUGENE, OREGON 97408
 (541) 344-5500
 www.arlie.com

ISSUED:
 10/14/2005 - TENTATIVE PUD
 05/11/2012 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION

STAMP

PROJECT # 20181700
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 CHECKED BY: MKK
 FILE NAME:
 DATE: 03/18/2013

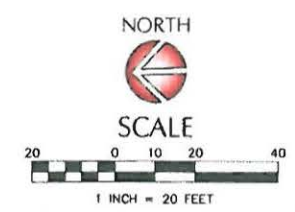
CANNERY STATION
 DRAWING
C2.0
 MASS-GRADING
 PLAN



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Community Development
Department

23
Exhibit

PC 1212 FPUD 01
File Number



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PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:



arlie&company

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ISSUED:
 10/11/2008 - TENTATIVE PUD
 05/17/2012 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION

STAMP:



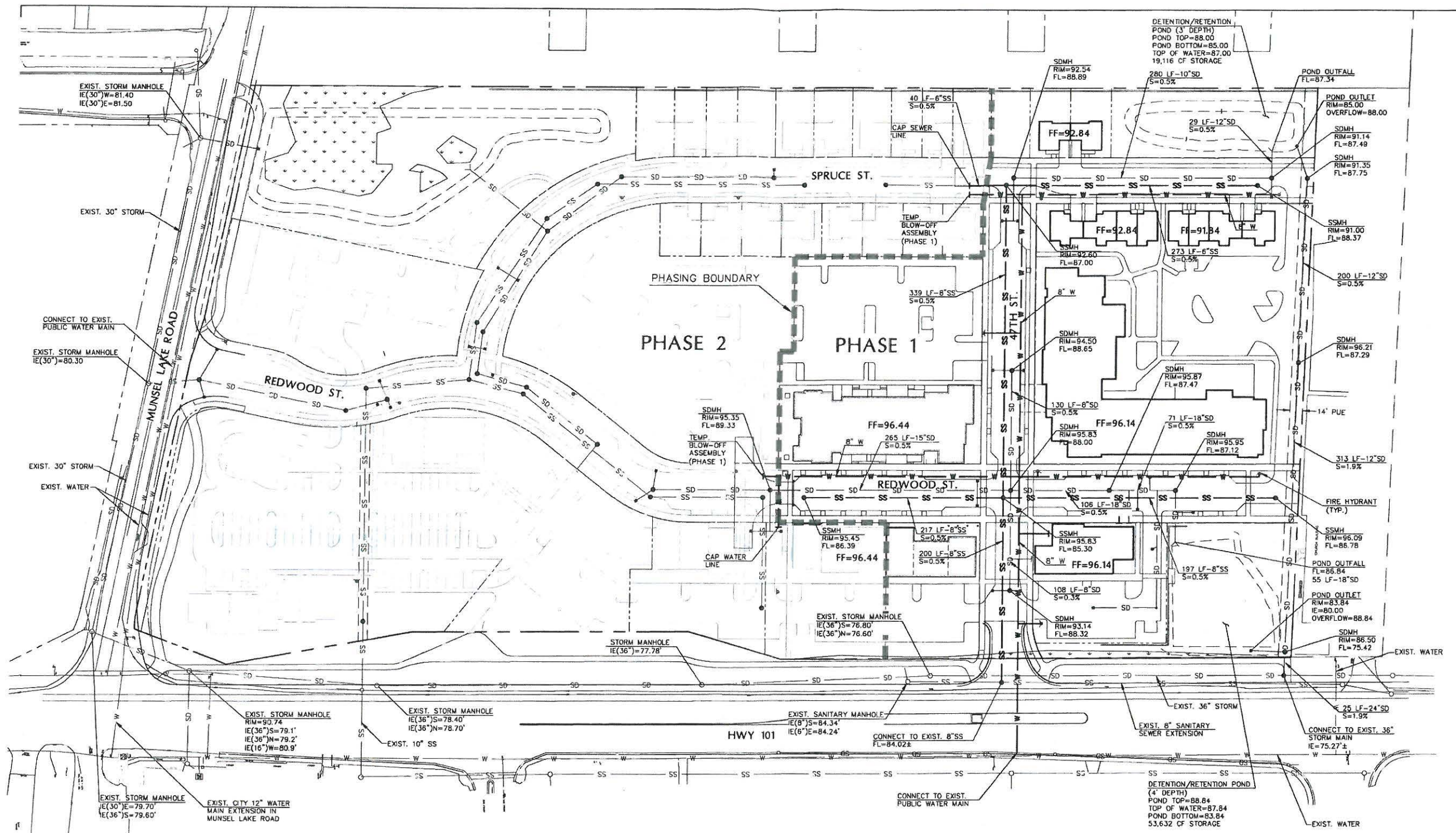
EXPIRATION DATE: 6/13/17

PROJECT # 307827.00
 DRAWN BY: AMF
 CHECKED BY: MK
 FILE NAME:
 DATE: 03/18/2013

CANNERY STATION

DRAWING:
C2.1

LOT 1
 GRADING PLAN



LEGEND

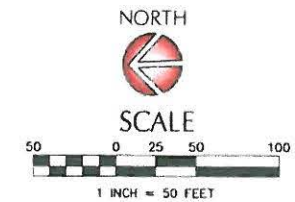
EXISTING	PROPOSED (PHASE 1)

ABBREVIATIONS

E	EAST
EXIST.	EXISTING
FF	FINISH FLOOR
FL	FLOW LINE
FM	FORCE MAIN
HWY	HIGHWAY
IE	INVERT ELEVATION
LF	LINEAL FEET
MH	MANHOLE
PUE	PUBLIC UTILITY EASEMENT
S	SLOPE OR SOUTH
SD	STORM DRAIN
SDMH	STORM DRAIN MANHOLE
SS	SANITARY SEWER
SSMH	SANITARY SEWER MANHOLE
ST.	STREET
TEMP.	TEMPORARY
TP	TOP OF PAVEMENT
WGMH	WATER QUALITY MANHOLE
W	WATER MAIN

NOTE:
 ALL UTILITIES SHOWN WITHIN THE ROADWAY ARE PROPOSED AS PUBLIC UTILITIES. EASEMENTS WILL BE GRANTED, AS APPROPRIATE, FOR UTILITIES IN ACCESS STREETS.
 THE WATER LINES ARE ASSUMED TO ALL BE 8-INCH FOR THE P.U.D. THE SIZING OF THESE PIPES WILL BE ADJUSTED AS NECESSARY PER SUBDIVISION REQUIREMENTS AND EXIST PRESSURE.

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 Community Development
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 PC1212FPUD01
 File Number



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PROJECT:
CANNERY STATION
 FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:

arlie@company

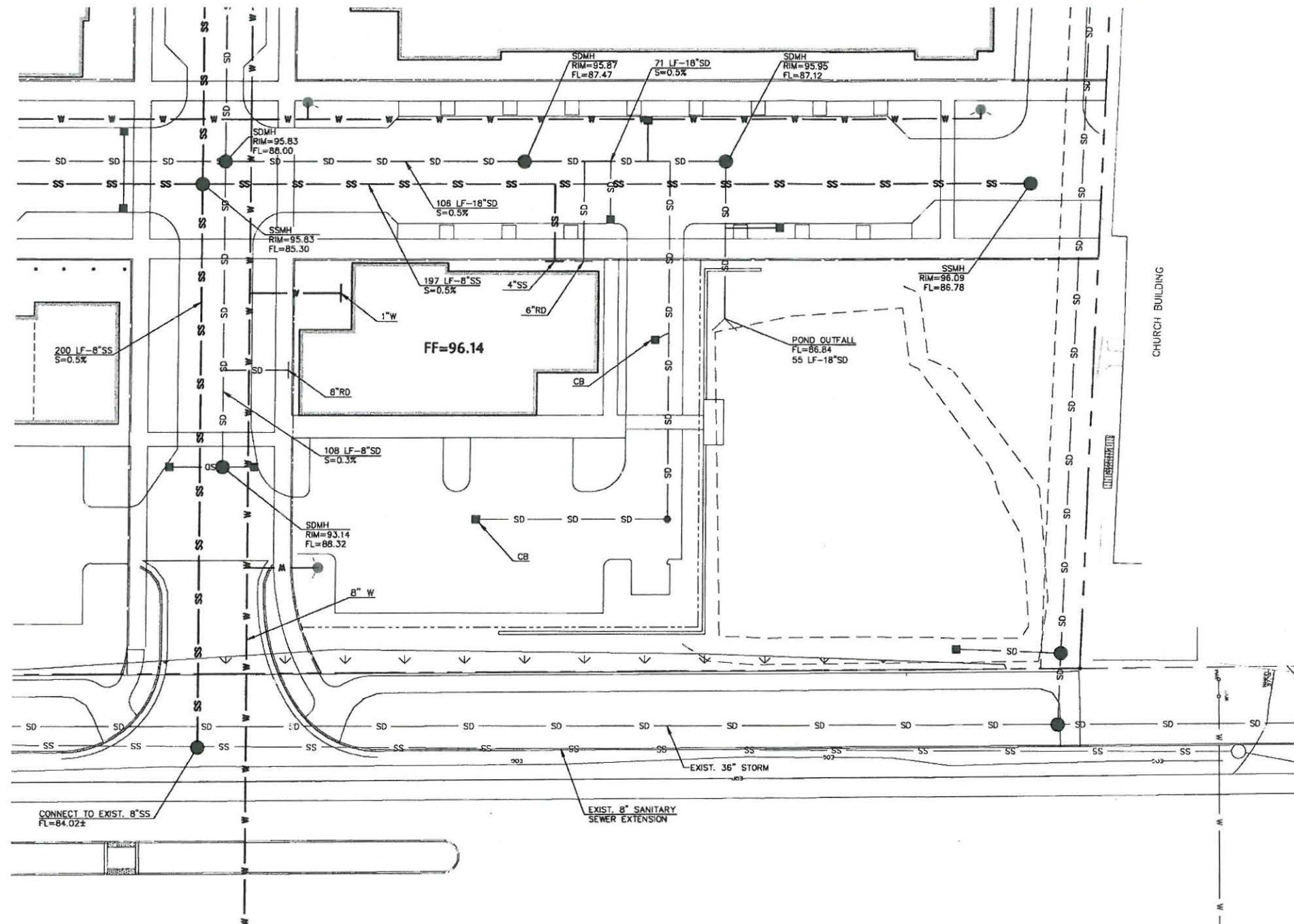
ARLIE & COMPANY
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ISSUED:
 10/14/2008 - TENTATIVE PUD
 04/17/2012 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION

STAMP:

 EXPIRATION DATE: 12/31/13
 PROJECT #: 30787.00
 DRAWN BY: JMF
 CHECKED BY: MIK
 FILE NAME:
 DATE: 03/18/2013

CANNERY STATION
 DRAWING:
C3.0
 UTILITY PLAN

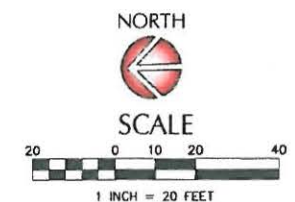


ABBREVIATIONS

CB	CATCH BASIN
EXIST.	EXISTING
FF	FINISH FLOOR
FL	FLOW LINE
FM	FORCE MAIN
Hwy	HIGHWAY
IE	INVERT ELEVATION
LF	LINEAL FEET
MH	MANHOLE
PUE	PUBLIC UTILITY EASEMENT
S	SLOPE OR SOUTH
SD	STORM DRAIN
SDMH	STORM DRAIN MANHOLE
SS	SANITARY SEWER
SSMH	SANITARY SEWER MANHOLE
ST.	STREET
TEMP.	TEMPORARY
TP	TOP OF PAVEMENT
WQMH	WATER QUALITY MANHOLE
W	WATER MAIN

NOTE:
 1. ELECTRICAL AND FRANCHISED UTILITIES ARE TO BE UNDERGROUND AND DESIGNED TO CENTRAL LINCOLN PEOPLE'S UTILITY DISTRICT, CHARTER COMMUNICATIONS, CENTURY LINK, ETC STANDARDS.

APPROVED
City of Florence
Community Development
Department
 25
 Exhibit
 PC 1212 FPU001
 File Number



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 ARCHITECT
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 Portland, Oregon 97210

kpff
 Consulting Engineers
 501 Oak Street
 Suite 200
 Eugene Oregon 97401
 541 684-4022
 FAX 541 684-4001

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:

ARLIE & COMPANY
 2511 TENNYSON AVENUE
 SUITE 400
 EUGENE, OREGON 97408
 (541) 344-5500
 www.arlie.com

ISSUED:
 10/14/2008 - TENTATIVE PUD
 05/17/2012 - FINAL PUD - PHASE 1
 02/18/2013 - REVISION

STAMP: REGISTERED PROFESSIONAL ENGINEER 55481

EXPIRATION DATE: 6/30/17
 PROJECT #: 207827.00
 DRAWN BY: AMF
 CHECKED BY: MK
 FILE NAME:
 DATE: 03/18/2013

CANNERY STATION

DRAWING:
C3.1
 LOT 1
 UTILITY PLAN

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 Portland, Oregon 97210



PROJECT:
CANNERY STATION
 FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6



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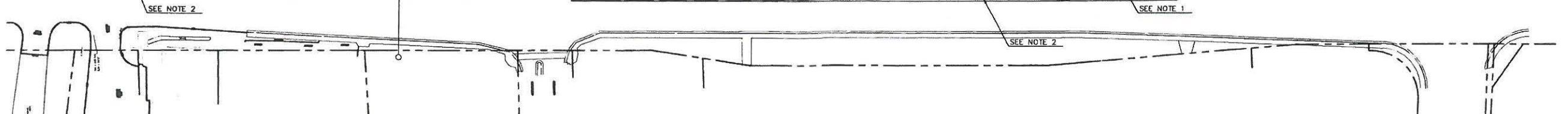
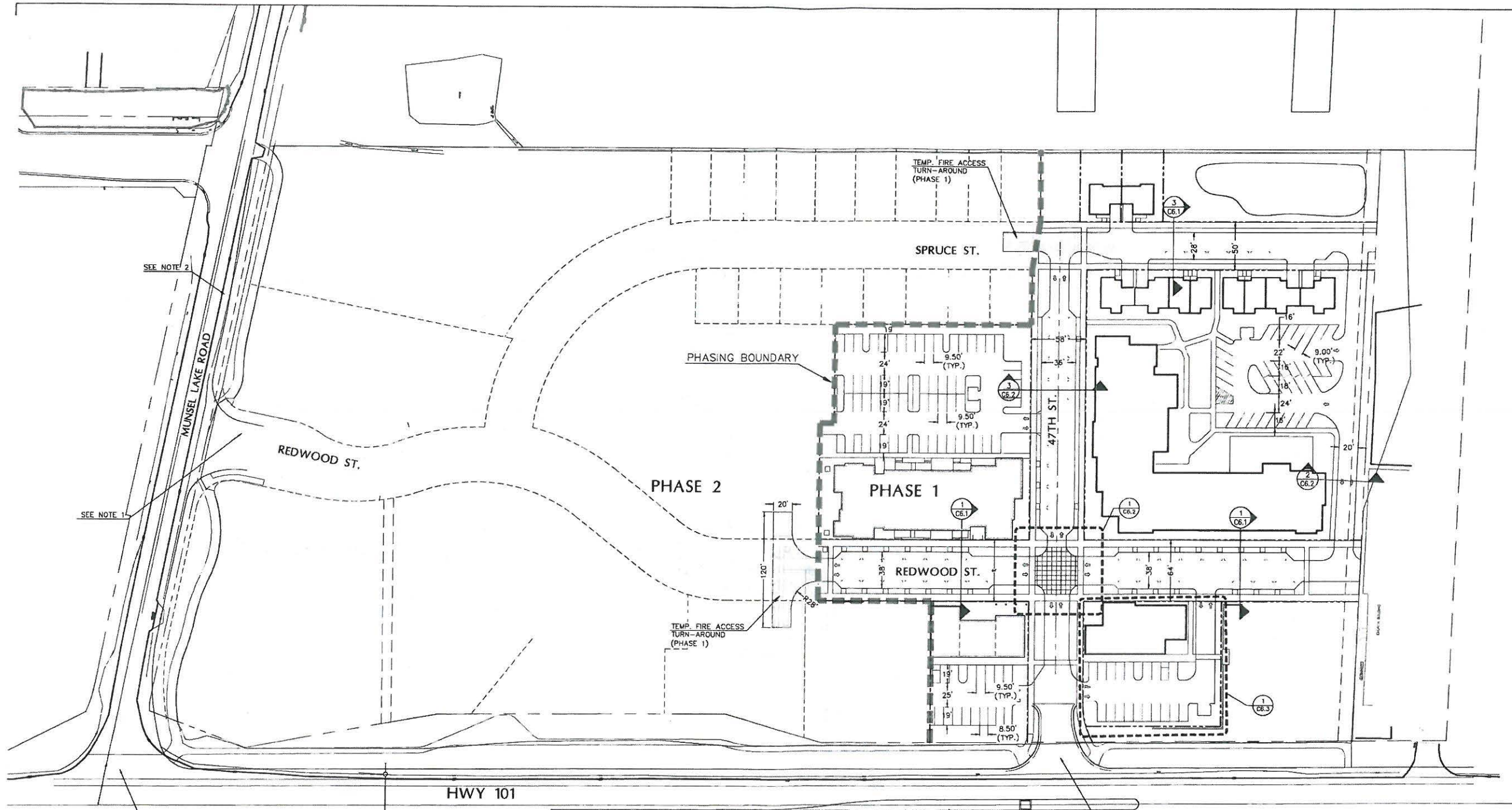


EXPIRATION DATE: 6/30/15
 PROJECT #: 207837.00
 DRAWN BY: AMF
 CHECKED BY: MK
 FILE NAME:
 DATE: 03/19/2013

CANNERY STATION

C6.0

STREET PLAN

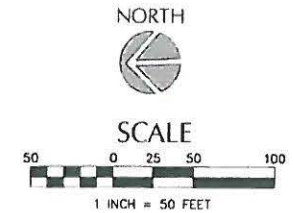


LEGEND	
EXISTING	PROPOSED
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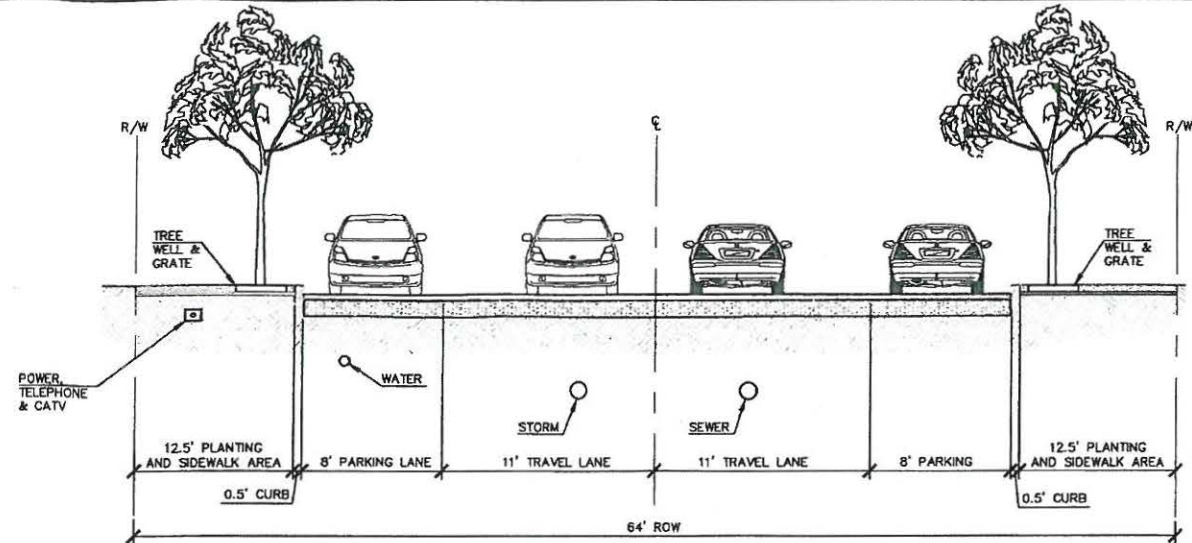
ABBREVIATIONS	
HWY	HIGHWAY
ST.	STREET

- NOTES:
- REFER TO TRANSPORTATION PLANS FOR INTERSECTION DESIGN INFORMATION.
 - REFER TO TRANSPORTATION PLANS FOR DESIGN INFORMATION RELATED TO HIGHWAY 101 AND MUNSEL LAKE ROAD.
 - REFER TO SHEET C6.3 FOR LOT 1 DETAIL.

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City of Florence
Community Development
Department
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 Exhibit
 PC1212 FPUD 01
 File Number



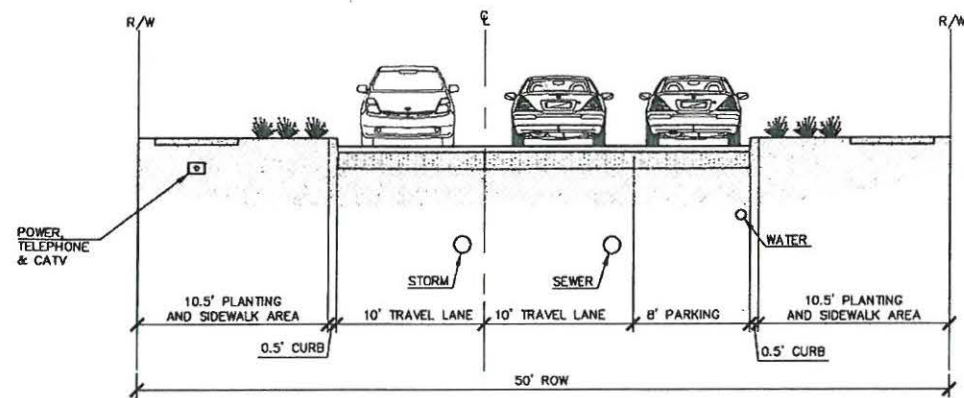
4
C6.1 NOT USED
N.T.S.



1
C6.1 REDWOOD STREET
38' STREET SECTION
N.T.S.

5
C6.1 NOT USED
N.T.S.

2
C6.1 NOT USED
N.T.S.



3
C6.1 SPRUCE STREET
28' STREET SECTION
N.T.S.

APPROVED
City of Florence
Community Development
Department
27 Exhibit
PC1212FPUD01
File Number

- NOTE:
1. ALL SIDEWALKS WILL BE A MINIMUM OF FIVE FEET IN WIDTH
 2. PLANTINGS SHOWN ARE PICTORIAL REPRESENTATIONS ONLY. PLANTING LOCATIONS AND TYPES (E.G. TREES) ARE PROVIDED ON THE LANDSCAPE PLANS.

6
C6.1 NOT USED
N.T.S.

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PROJECT:
CANNERY STATION
FLORENCE, OR
FINAL PUD - PHASE 1
LOTS 1 - 6

CLIENT:

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10/14/2008 - TENTATIVE PUD
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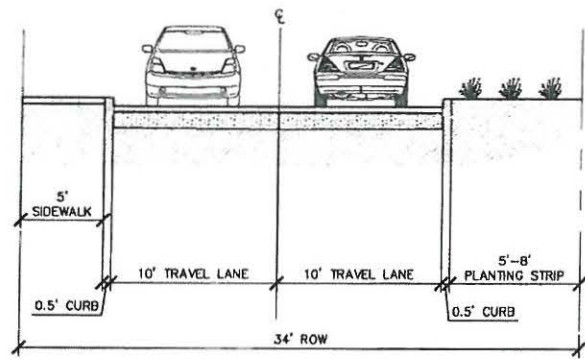
STAMP:
REGISTERED PROFESSIONAL
ENGINEER
56481
OREGON
JUL 15 2008
MATTHEW N. KEENAN

EXPIRATION DATE: 6/30/17
PROJECT #: 307827/00
DRAWN BY: AMF
CHECKED BY: MK
FILE NAME:
DATE: 03/18/2013

CANNERY STATION
DRAWING:

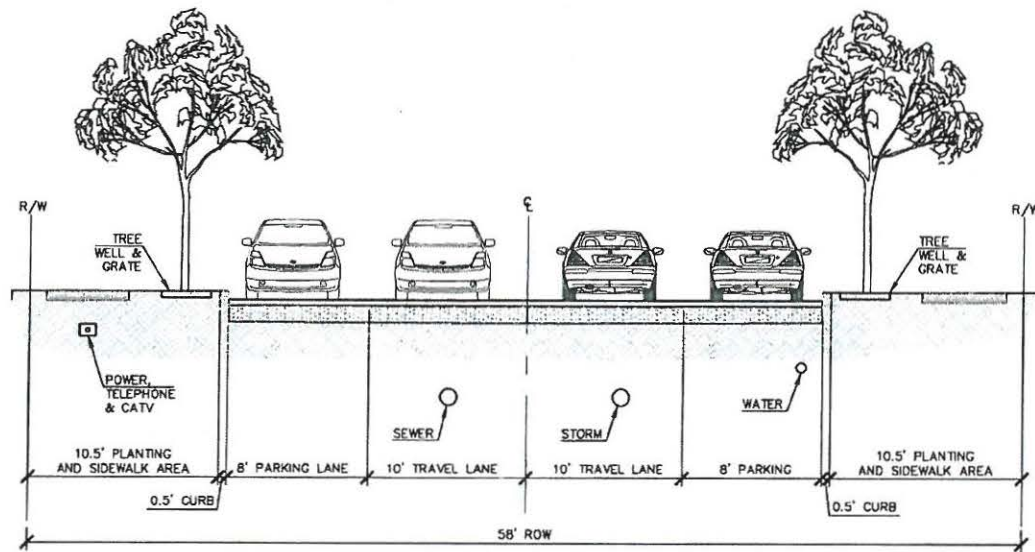
C6.1

STREET
TYPICAL SECTIONS



2
C6.2

PRIVATE ACCESS W/
PUBLIC ACCESS EASEMENT
20' STREET SECTION
N.T.S.

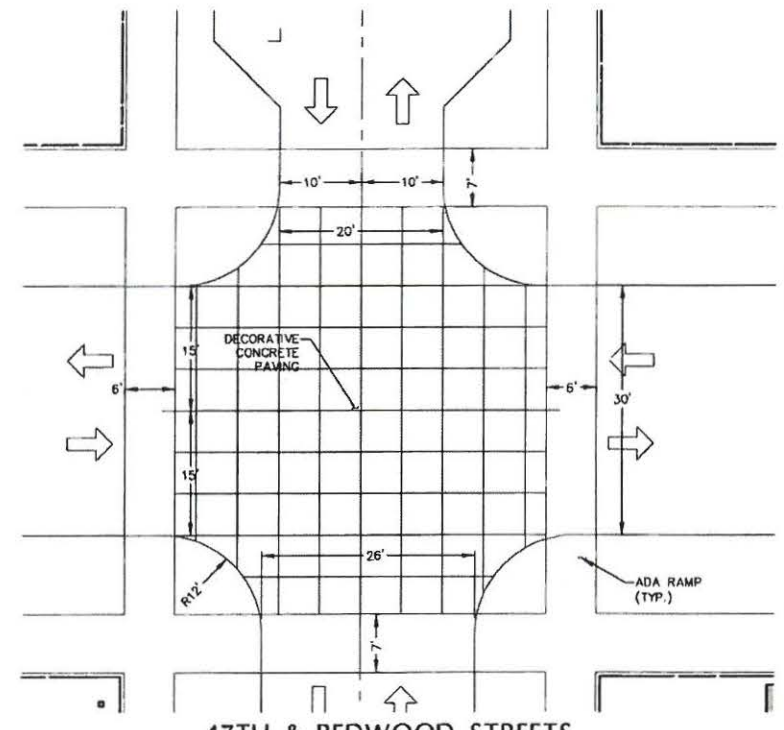


3
C6.2

47TH STREET
36' STREET SECTION
N.T.S.

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Community Development
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28 Exhibit
PC1212FPUD 01 File Number

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1
C6.2

47TH & REDWOOD STREETS
INTERSECTION DETAIL
1" = 10'

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FLORENCE, OR

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LOTS 1 - 6



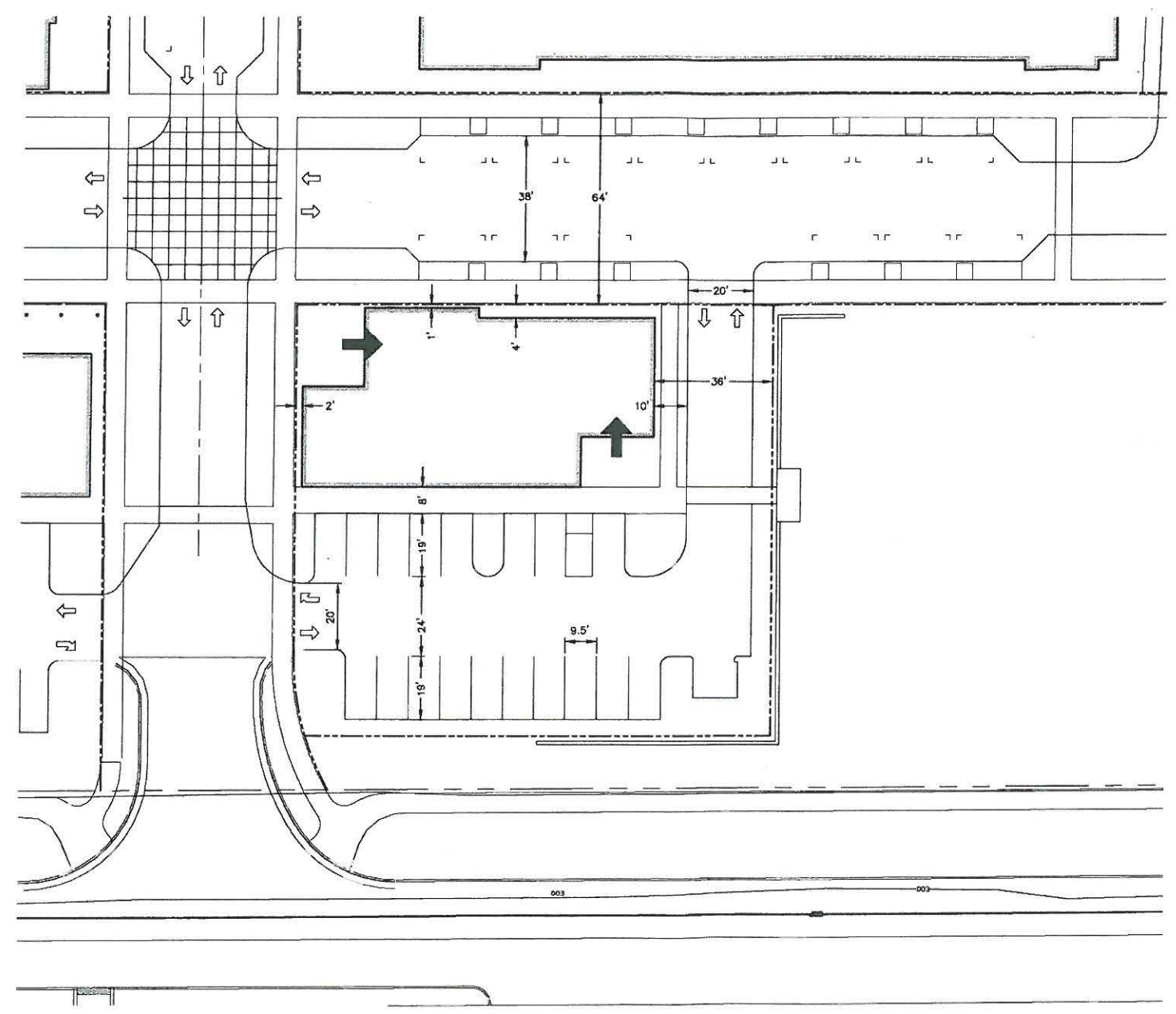
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ISSUED:
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06/17/2012 - FINAL PUD - PHASE 1
03/18/2013 - REVISION



EXPIRATION DATE: 6/30/17
PROJECT #: 307827-00
DRAWN BY: AJMF
CHECKED BY: NIK
FILE NAME:
DATE: 03/18/2013

CANNERY STATION
DRAWING:
C6.2
STREET
INTERSECTION
DETAILS/SECTIONS



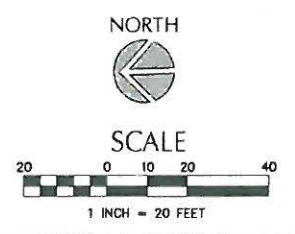
1
C6.3
LOT 1 DETAIL
1" = 20'



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Community Development
Department

29
Exhibit

PC1212 FPUD 01
File Number



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PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
LOTS 1 - 6



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02/18/2013 - REVISION



PROJECT #: 307827.00
DRAWN BY: AJMF
CHECKED BY: MKK
FILE NAME:
DATE: 02/18/2013

CANNERY STATION
DRAWING:

C6.3

LOT 1
CIVIL SITE PLAN

EROSION AND SEDIMENT CONTROL PLANS

THE PERMITEE IS REQUIRED TO MEET ALL THE CONDITIONS OF THE 1200C PERMIT. THIS ESCP AND GENERAL CONDITIONS HAVE BEEN DEVELOPED TO FACILITATE COMPLIANCE WITH THE 1200C PERMIT REQUIREMENTS. IN CASES OF DISCREPANCIES OR OMISSIONS, THE 1200C PERMIT REQUIREMENTS SUPERCEDE REQUIREMENTS OF THIS PLAN.

MARCH 2013

DEQ STANDARD EROSION AND SEDIMENT CONTROL PLAN DRAWING NOTES

- HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS. (SCHEDULE A.8.C.I.(3))
- ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200-C PERMIT REQUIREMENTS.
- INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.
- RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. DURING INACTIVE PERIODS OF GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS, RETAIN THE ESCP AT THE CONSTRUCTION SITE OR AT ANOTHER LOCATION. (SCHEDULE B.2.A)
- ALL PERMIT REGISTRANTS MUST IMPLEMENT THE ESCP. FAILURE TO IMPLEMENT ANY OF THE CONTROL MEASURES OR PRACTICES DESCRIBED IN THE ESCP IS A VIOLATION OF THE PERMIT. (SCHEDULE A.8.A)
- THE ESCP MEASURES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, UPGRADE THESE MEASURES AS NEEDED TO COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL EROSION AND SEDIMENT CONTROL REGULATIONS. (SCHEDULE A.8.C.I.(1)(C))
- SUBMISSION OF ALL ESCP REVISIONS IS NOT REQUIRED. SUBMITTAL OF THE ESCP REVISIONS IS ONLY UNDER SPECIFIC CONDITIONS. SUBMIT ALL NECESSARY REVISION TO DEQ OR AGENT. (SCHEDULE A.12.C.II)
- PHASE CLEARING AND GRADING TO THE MAXIMUM EXTENT PRACTICAL TO PREVENT EXPOSED INACTIVE AREAS FROM BECOMING A SOURCE OF EROSION. (SCHEDULE A.8.C.I.(1)(D))
- IDENTIFY, MARK, AND PROTECT (BY FENCING OFF OR OTHER MEANS) CRITICAL RIPARIAN AREAS AND VEGETATION INCLUDING IMPORTANT TREES AND ASSOCIATED ROOTING ZONES, AND VEGETATION AREAS TO BE PRESERVED. IDENTIFY VEGETATIVE BUFFER ZONES BETWEEN THE SITE AND SENSITIVE AREAS (E.G., WETLANDS), AND OTHER AREAS TO BE PRESERVED, ESPECIALLY IN PERIMETER AREAS. (SCHEDULE A.8.C.I.(1) & (2))
- PRESERVE EXISTING VEGETATION WHEN PRACTICAL AND RE-VEGETATE OPEN AREAS. RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION. IDENTIFY THE TYPE OF VEGETATIVE SEED MIX USED. (SCHEDULE A.7.B.III(1) AND A.7.B.III(3))
- EROSION AND SEDIMENT CONTROL MEASURES INCLUDING PERIMETER SEDIMENT CONTROL MUST BE IN PLACE BEFORE VEGETATION IS DISTURBED AND MUST REMAIN IN PLACE AND BE MAINTAINED, REPAIRED, AND PROMPTLY IMPLEMENTED FOLLOWING PROCEDURES ESTABLISHED FOR THE DURATION OF CONSTRUCTION, INCLUDING PROTECTION FOR ACTIVE STORM DRAIN INLETS AND CATCH BASINS, AND APPROPRIATE NON-STORMWATER POLLUTION CONTROLS. (SCHEDULE A.7.D.I AND A.8.C)
- ESTABLISH CONCRETE TRUCK AND OTHER CONCRETE EQUIPMENT WASHOUT AREAS BEFORE BEGINNING CONCRETE WORK. (SCHEDULE A.8.C.I.(6))
- APPLY TEMPORARY AND/OR PERMANENT SOIL STABILIZATION MEASURES IMMEDIATELY ON ALL DISTURBED AREAS AS GRADING PROGRESSES AND FOR ALL ROADWAYS INCLUDING GRAVEL ROADWAYS. (SCHEDULE A.8.C.II.(2))
- ESTABLISH MATERIAL AND WASTE STORAGE AREAS, AND OTHER NON-STORMWATER CONTROLS. (SCHEDULE A.8.C.I.(7))
- PREVENT TRACKING OF SEDIMENT ONTO PUBLIC OR PRIVATE ROADS USING BMPs SUCH AS GRAVELED (OR PAVED) EXITS AND PARKING AREAS, GRAVEL ALL UNPAVED ROADS LOCATED ON-SITE, OR USE AN EXIT TIRE WASH. THESE BMPs MUST BE IN PLACE PRIOR TO LAND-DISTURBING ACTIVITIES. (SCHEDULE A.7.D.II.(1) AND A.8.C.I.(4))
- WHEN TRUCKING SATURATED SOILS FROM THE SITE, EITHER USE WATER-TIGHT TRUCKS OR DRAIN LOADS ON SITE. (SCHEDULE A.7.D.II.(3))
- USE BMPs TO PREVENT OR MINIMIZE STORMWATER EXPOSURE TO POLLUTANTS FROM SPILLS; VEHICLE AND EQUIPMENT FUELING, MAINTENANCE, AND STORAGE; OTHER CLEANING AND MAINTENANCE ACTIVITIES, AND WASTE HANDLING ACTIVITIES. THESE POLLUTANTS INCLUDE FUEL, HYDRAULIC FLUID, AND OTHER OILS FROM VEHICLES AND MACHINERY, AS WELL AS DEBRIS, LEFTOVER PAINTS, SOLVENTS, AND GLUES FROM CONSTRUCTION OPERATIONS. (SCHEDULE A.7.E.I.(2))
- IMPLEMENT THE FOLLOWING BMPs WHEN APPLICABLE: WRITTEN SPILL PREVENTION AND RESPONSE PROCEDURES, EMPLOYEE TRAINING ON SPILL PREVENTION AND PROPER DISPOSAL PROCEDURES; SPILL KITS IN ALL VEHICLES, REGULAR MAINTENANCE SCHEDULE FOR VEHICLES AND MACHINERY; MATERIAL DELIVERY AND STORAGE CONTROLS; TRAINING AND SIGNAGE, AND COVERED STORAGE AREAS FOR WASTE AND SUPPLIES. (SCH A.7.E.II.)
- USE WATER, SOIL-BINDING AGENT OR OTHER DUST CONTROL TECHNIQUE AS NEEDED TO AVOID WIND-BLOWN SOIL. (SCHEDULE A.7.B.II)
- THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS TO MINIMIZE NUTRIENT RELEASES TO SURFACE WATERS. EXERCISE CAUTION WHEN USING TIME-RELEASE FERTILIZERS WITHIN ANY WATERWAY RIPARIAN ZONE. (SCHEDULE A.9.B.III)
- IF A STORMWATER TREATMENT SYSTEM (FOR EXAMPLE, ELECTRO-COAGULATION, FLOCCULATION, FILTRATION, ETC.) FOR SEDIMENT OR OTHER POLLUTANT REMOVAL IS EMPLOYED, SUBMIT AN OPERATION AND MAINTENANCE PLAN (INCLUDING SYSTEM SCHEMATIC, LOCATION OF SYSTEM, LOCATION OF INLET, LOCATION OF DISCHARGE, DISCHARGE DISPERSION DEVICE DESIGN, AND A SAMPLING PLAN AND FREQUENCY) BEFORE OPERATING THE TREATMENT SYSTEM. OBTAIN PLAN APPROVAL BEFORE OPERATING THE TREATMENT SYSTEM. OPERATE AND MAINTAIN THE TREATMENT SYSTEM ACCORDING TO MANUFACTURER'S SPECIFICATIONS. (SCHEDULE A.9.D)
- TEMPORARILY STABILIZE SOILS AT THE END OF THE SHIFT BEFORE HOLIDAYS AND WEEKENDS, IF NEEDED. THE REGISTRANT IS RESPONSIBLE FOR ENSURING THAT SOILS ARE STABLE DURING RAIN EVENTS AT ALL TIMES OF THE YEAR. (SCHEDULE A.7.B)
- AT THE END OF EACH WORKDAY SOIL STOCKPILES MUST BE STABILIZED OR COVERED, OR OTHER BMPs MUST BE IMPLEMENTED TO PREVENT DISCHARGES TO SURFACE WATERS OR CONVEYANCE SYSTEMS LEADING TO SURFACE WATERS. (SCHEDULE A.7.E.II.(2))
- CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND DURING WET WEATHER. (SCHEDULE A.7.A.I)
- SEDIMENT FENCE: REMOVE TRAPPED SEDIMENT BEFORE IT REACHES ONE THIRD OF THE ABOVE GROUND FENCE HEIGHT AND BEFORE FENCE REMOVAL. (SCHEDULE A.9.C.I)

- OTHER SEDIMENT BARRIERS (SUCH AS BIOBAGS): REMOVE SEDIMENT BEFORE IT REACHES TWO INCHES DEPTH ABOVE GROUND HEIGHT, AND BEFORE BMP REMOVAL. (SCHEDULE A.9.C.II)
- CATCH BASINS: CLEAN BEFORE RETENTION CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT. SEDIMENT BASINS AND SEDIMENT TRAPS: REMOVE TRAPPED SEDIMENTS BEFORE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT AND AT COMPLETION OF PROJECT. (SCHEDULE A.9.C.II & IV)
- WITHIN 24 HOURS, SIGNIFICANT SEDIMENT THAT HAS LEFT THE CONSTRUCTION SITE, MUST BE REMEDIATED. INVESTIGATE THE CAUSE OF THE SEDIMENT RELEASE AND IMPLEMENT STEPS TO PREVENT A RECURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DIVISION OF STATE LANDS REQUIRED TIMEFRAME. (SCHEDULE A.9.B.I)
- THE INTENTIONAL WASHING OF SEDIMENT INTO STORM SEWERS OR DRAINAGE WAYS MUST NOT OCCUR. VACUUMING OR DRY SWEEPING AND MATERIAL PICKUP MUST BE USED TO CLEANUP RELEASED SEDIMENTS. (SCHEDULE A.9.B.II)
- THE ENTIRE SITE MUST BE TEMPORARILY STABILIZED USING VEGETATION OR A HEAVY MULCH LAYER, TEMPORARY SEEDING, OR OTHER METHOD SHOULD ALL CONSTRUCTION ACTIVITIES CEASE FOR 30 DAYS OR MORE. (SCHEDULE A.7.F.I)
- PROVIDE TEMPORARY STABILIZATION FOR THAT PORTION OF THE SITE WHERE CONSTRUCTION ACTIVITIES CEASE FOR 14 DAYS OR MORE WITH A COVERING OF BLOWN STRAW AND A TACKIFIER, LOOSE STRAW, OR AN ADEQUATE COVERING OF COMPOST MULCH UNTIL WORK RESUMES ON THAT PORTION OF THE SITE. (SCHEDULE A.7.F.II)
- PROVIDE PERMANENT EROSION CONTROL MEASURES ON ALL EXPOSED AREAS. DO NOT REMOVE TEMPORARY SEDIMENT CONTROL PRACTICES UNTIL PERMANENT VEGETATION OR OTHER COVER OF EXPOSED AREAS IS ESTABLISHED. HOWEVER, DO REMOVE ALL TEMPORARY EROSION CONTROL MEASURES AS EXPOSED AREAS BECOME STABILIZED, UNLESS DOING SO CONFLICTS WITH LOCAL REQUIREMENTS. PROPERLY DISPOSE OF CONSTRUCTION MATERIALS AND WASTE, INCLUDING SEDIMENT RETAINED BY TEMPORARY BMPs. (SCHEDULE A.7.B.III(2) AND A.8.C.II)

BMP MATRIX FOR CONSTRUCTION PHASES

REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'S

	CLEARING	MASS GRADING	UTILITY INSTALLATION	FINAL STABILIZATION
EROSION PREVENTION				
GROUND COVER				X
PLASTIC SHEETING				X
DUST CONTROL	X	X	X	X
TEMP./PERM. SEEDING		X		X
SEDIMENT CONTROL				
SEDIMENT FENCE (PERIMETER)	X	X	X	X
STRAW WATTLES	X	X	X	X
INLET PROTECTION	X	X	X	X
SEDIMENT TRAP				X
RUN OFF CONTROL				
CONSTRUCTION ENTRANCE	X	X	X	X
PIPE SLOPE DRAIN				X
OUTLET PROTECTION				X
POLLUTION PREVENTION				
PROPER SIGNAGE	X	X	X	X
HAZ WASTE MGMT	X	X	X	X
SPILL KIT ON-SITE	X	X	X	X
CONCRETE WASHOUT AREA	X	X	X	X
OTHER				
WET WEATHER	X	X	X	X
DE-WATERING				X
RATIONALE STATEMENT				
A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S 1200-C PERMIT APPLICATION AND ESCP GUIDANCE DOCUMENT HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMPs WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOILS CONDITIONS, TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS. AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESCP, AN ACTION PLAN WILL BE SUBMITTED.				
INITIAL _____				

ATTENTION EXCAVATORS

OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 852-001-0010 THROUGH OAR 852-001-0080. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING 503-232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST TWO BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL 503-246-6699.

GENERAL NOTES

- THESE DOCUMENTS WERE PREPARED SOLELY FOR THE USE OF EROSION CONTROL ON THE CANNERY STATION PROJECT.
- THE EXISTING AND PROPOSED CONTOURS SHOWN ARE FOR REFERENCE ONLY AND MAY NOT ACCURATELY REFLECT THE FIELD CONDITIONS.
- ANY ITEM NOTED BY OTHERS IS DEPICTED ON A SEPARATE SET OF CONSTRUCTION DOCUMENTS NOT PREPARED BY KPFF CONSULTING ENGINEERS, INC.
- THE CONTRACTOR MAY ALTER THE LOCATION AND TYPE OF EROSION CONTROL MEASURES SHOWN BASED ON FIELD CONDITIONS WITH APPROVAL BY THE CITY OF FLORENCE.

DEVELOPER

NAME: ARLIE & COMPANY

CONTACT: TERESA BISHOP
ADDRESS: 2911 TENNYSON AVENUE, SUITE 400
EUGENE, OR 97408

PHONE: (541) 344-5500
E-MAIL: teresa@arlie.com

ENGINEERING / SURVEYING FIRM

KPFF CONSULTING ENGINEERS
CONTACT: MATT KEENAN
1201 OAK STREET
EUGENE, OR 97401
PHONE: 541-684-4902
FAX: 503-684-4909

WORBE & ASSOCIATES, INC.
CONTACT: EUGENE WOBBE
510 KINGWOOD ST
P.O. BOX 3093
FLORENCE, OR 97439
PHONE: 541-997-8411

FLOOD NOTE

THE RIGHT-OF-WAY FOR 13TH AVE. IS WITHIN FLOOD ZONE AE, WHICH IS AN AREA OF THE 100-YEAR FLOOD, PER FEMA FLOOD ZONE MAP 41039C1137 F. THE REMAINDER OF THE SITE IS OUTSIDE OF THE FLOOD ZONE. ALL THE AREAS DISTURBED BY THIS PROJECT ARE ABOVE THE BASE FLOOD ELEVATION.

ABBREVIATIONS

BMP BEST MANAGEMENT PRACTICES
DEQ DEPARTMENT OF ENVIRONMENTAL QUALITY
FOR THE STATE OF OREGON
ESC EROSION AND SEDIMENT CONTROL
ESCP EROSION AND SEDIMENT CONTROL PLAN
EXIST. EXISTING

EXISTING SITE CONDITIONS

NATURAL AREA

DEVELOPED CONDITIONS

COMMERCIAL SUBDIVISION

NATURE OF CONSTRUCTION ACTIVITY AND ESTIMATED TIME TABLE

- PHASE I
- CLEARING (6/13-7/13)
 - MASS GRADING (6/13-9/13)
 - ROAD CONSTRUCTION (7/14-9/14)
 - FINAL STABILIZATION (9/14-10/14)

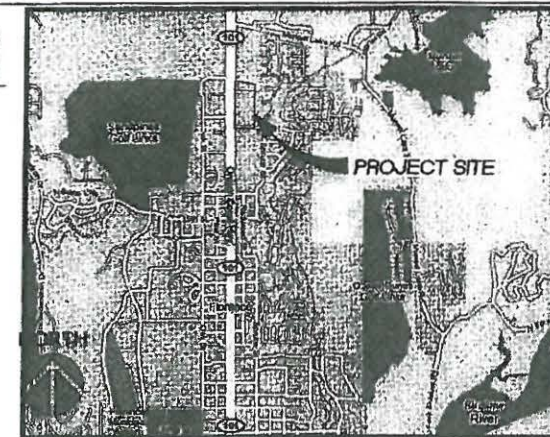
TOTAL SITE AREA = TOTAL DISTURBED AREA = 735,038 SF = 16.87 ACRES

SITE SOIL CLASSIFICATION:

131C/131E - WALDPOR FINE SAND (TYPE A)
29 - YAQUINA LOAMY FINE SAND (TYPE D)

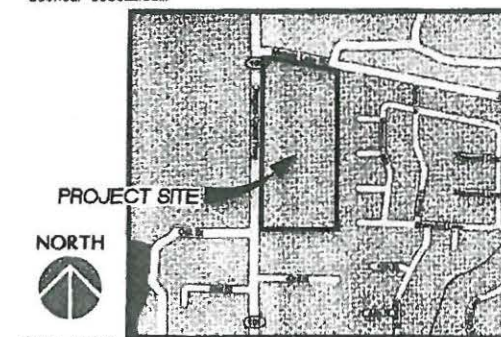
RECEIVING WATER BODIES:

MUNSEL CREEK
PACIFIC OCEAN



VICINITY MAP

SCALE: NTS
SOURCE: GOOGLE.COM



SITE MAP

SCALE: NTS
SOURCE: GOOGLE.COM

TONY KOACH
ARCHITECT
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Portland, Oregon 97210



PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
LOTS 1-8



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ISSUED:
10/14/2008 - TENTATIVE PUD
09/17/2013 - FINAL PUD - PHASE 1
09/18/2013 - REVISION

PERMITEE'S SITE INSPECTOR

NAME: MATT KEENAN
COMPANY/AGENCY: KPFF CONSULTING, INC.
PHONE: 541-684-4902
FAX: 541-684-4909
E-MAIL: MATT.KEENAN@KPFFCIVIL.COM

DESCRIPTION OF EXPERIENCE:

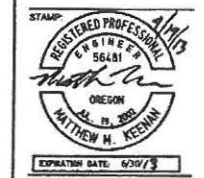
INSPECTION FREQUENCY

SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOWMELT, IS OCCURRING.
2. PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY.	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
3. INACTIVE PERIODS GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS.	ONCE EVERY TWO (2) WEEKS.
4. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.

- ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.
- INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.
- CHANGES TO THE APPROVED ESC PLAN MUST BE SUBMITTED TO DEQ IN THE FORM OF AN ACTION PLAN.

SHEET INDEX

NO.	SHEET	TITLE
1	EC1.0	COVER SHEET
2	EC2.0	EROSION CONTROL PLAN
3	EC3.0	EROSION CONTROL DETAILS



PROJECT #: 30727.00
DRAWN BY: AMF
CHECKED BY: MK
FILE NAME:
DATE: 03/18/2013

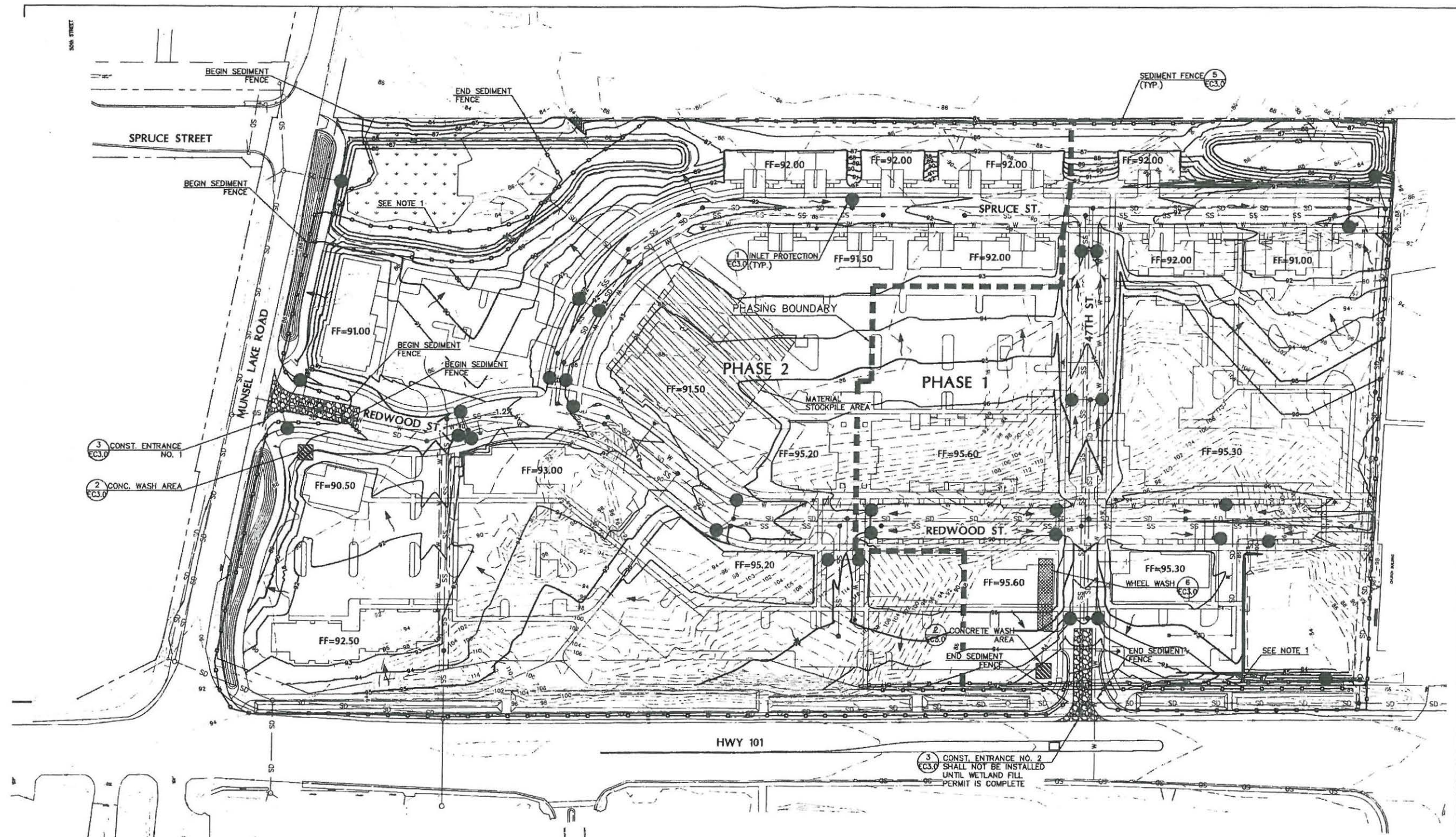
CANNERY STATION

EC1.0

EROSION CONTROL COVER SHEET

Exhibit 30

APPROVED
City of Florence
Community Development
Department
30 Exhibit
PC1212 FPUD 01
File Number



- 3 CONST. ENTRANCE NO. 1
- 2 CONC. WASH AREA

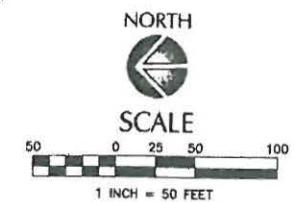
3 CONST. ENTRANCE NO. 2 SHALL NOT BE INSTALLED UNTIL WETLAND FILL PERMIT IS COMPLETE

EXISTING	LEGEND	PROPOSED
	PROPERTY LINE/ RIGHT-OF-WAY	
	WETLAND	
	MINOR CONTOUR	
	MAJOR CONTOUR	
	TOP/BOTTOM OF POND	
	SEDIMENT FENCE	

ABBREVIATIONS	
FF	FINISH FLOOR
HWY	HIGHWAY
HP	HIGH POINT
LP	LOW POINT
ST.	STREET
TP	TOP OF PAVEMENT

- NOTE:
1. SURVEY WETLAND BOUNDARY PRIOR TO CONSTRUCTION AND INSTALL SEDIMENT FENCE AT OUTSIDE EXTENTS OF WETLAND. WETLAND SHALL BE PROTECTED PRIOR TO WETLAND FILL PERMIT APPROVAL. FENCE MAY BE REMOVED AFTER WETLAND FILL PERMIT IS COMPLETE.
 2. SEE RE-VEGETATION PLAN FOR TEMPORARY AND PERMANENT SEEDING OF DISTURBED AREAS.

APPROVED
City of Florence
Community Development
Department
 31 Exhibit
 1212 FPUD01
 File Number



TONY KOACH
 ARCHITECT
 2301 N.W. Thurman
 Suite K
 Portland, Oregon 97210

kpff
 Consulting Engineers
 855 Oak Street
 Suite 220
 Eugene, Oregon 97401
 (541) 684-4552
 FAX 541 684-4558

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:

arlie@company

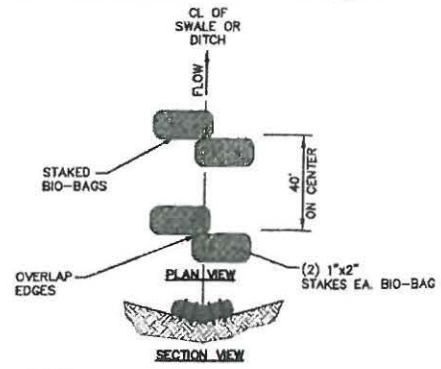
ARLIE & COMPANY
 2811 TENNYSON AVENUE
 SUITE 400
 EUGENE, OREGON 97408
 (541) 344-5500
 www.arlie.com

ISSUED:
 10/14/2008 - TENTATIVE PUD
 05/17/2012 - FINAL PUD - PHASE 1
 03/19/2013 - REVISION

EXPIRATION DATE: 07/31/13
 PROJECT #: 30787.00
 DRAWN BY: AMF
 CHECKED BY: MKK
 FILE NAME:
 DATE: 03/19/2013

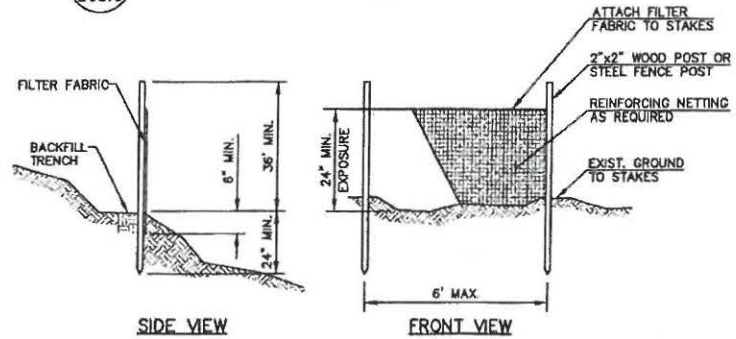
CANNERY STATION
 DRAWING:
EC2.0

EROSION CONTROL
 PLAN
 2 of 3



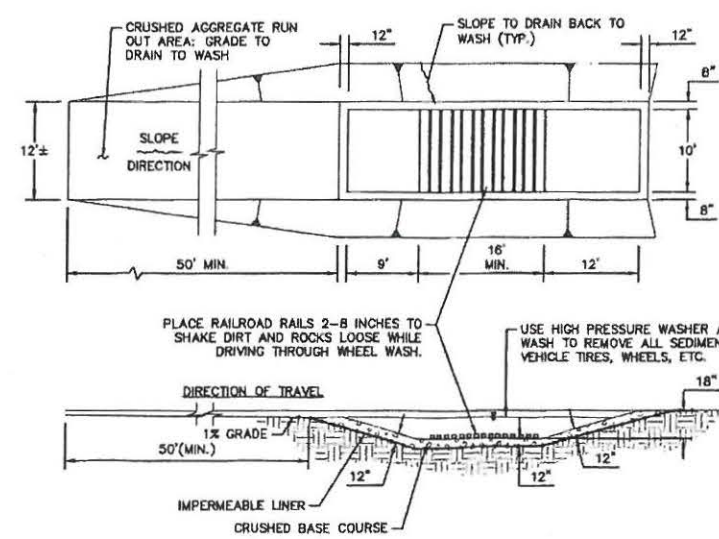
NOTES:
 1. STAKING OF BAGS REQUIRED USING (2) 1" x 2" WOOD STAKES OR APPROVED EQUAL PER BAG.
 2. DRIVE STAKES MINIMUM 12" INTO GROUND SURFACE.

4 BIO-BAG CHECK DAMS
 EC3.0 N.T.S.



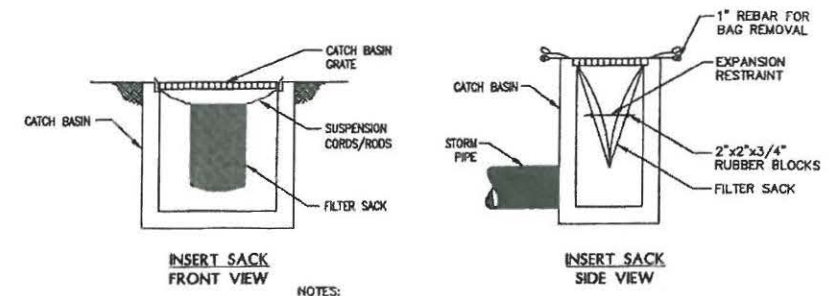
NOTES:
 1. INSTALLATION SHALL FOLLOW THE MANUFACTURER'S RECOMMENDATIONS.
 2. FILTER FABRIC - USE MINIMUM OF 36" WIDE ROLLS.

5 SEDIMENT FENCE
 EC3.0 N.T.S.



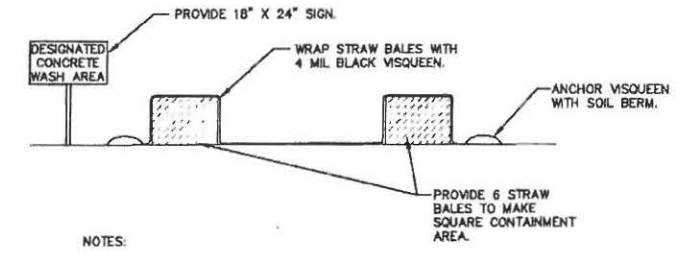
NOTES:
 1. CONTRACTOR SHALL PROVIDE SUMP PUMP, ABOVE GROUND PIPE, AND LARGE SIZE ORIFICE SPRINKLER TO SPREAD OVER EXISTING VEGETATION. SUBMIT DESIGNATED SPRINKLER AREA TO ENGINEER FOR APPROVAL.
 2. CONTRACTOR TO MAINTAIN WHEELWASH DAILY BY REMOVING ACCUMULATED SEDIMENT AND SEDIMENT LADEN WATER FROM WHEEL WASH WHEN ACCUMULATION REACHES 50% OF VOLUME. THEN FILL WITH WATER TO MAINTAIN 12" OF WATER IN WHEELWASH AT ALL TIMES.
 3. CONTRACTOR RESPONSIBLE FOR ENSURING PROPER PERFORMANCE OF THIS ESC MEASURE INCLUDING DRAINAGE TO AND AWAY FROM MEASURE.
 4. CONTRACTOR TO REPAIR IMMEDIATELY ANY PROBLEMS PREVENTING PROPER PERFORMANCE OF THIS ESC MEASURE OR ITS UPSTREAM/ DOWNSTREAM CONVEYANCE STRUCTURES.
 5. SEDIMENT REMOVED SHALL BE DISPOSED OF ON-SITE IN SOIL STOCKPILE AREA FOR RE-USE OR OFF-SITE TO A CITY APPROVED DISPOSAL FACILITY.

6 TEMPORARY WHEEL WASH
 EC3.0 N.T.S.



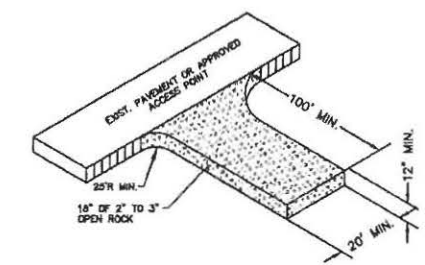
NOTES:
 1. INSTALL FILTER FABRIC INLET PROTECTION ON ALL NEW INLETS CONSTRUCTED ON PROJECT.

1 INLET PROTECTION
 EC3.0 N.T.S.



NOTES:
 1. INSTALL A CONCRETE WASH OUT, CONSTRUCTED WITH STRAW BALES, LINED WITH PLASTIC, AND A VISIBLE SIGN STATING "DESIGNATED CONCRETE WASH AREA." LOCATE THE WASH OUT IN A PLACE THAT WILL BE ACCESSIBLE TO CONCRETE TRUCKS SIZE TO THE PROJECT.

2 CONCRETE WASH AREA DETAIL
 EC3.0 N.T.S.



3 CONSTRUCTION ENTRANCE
 EC3.0 N.T.S.

APPROVED
City of Florence
Community Development
Department
 32 Exhibit
 PC1212FPUD01
 File Number

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 FAX (541) 684-4829

PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:

 arlie@company

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 (541) 344-5500
 www.arlie.com

ISSUED:
 10/14/2004 - TENTATIVE PUD
 05/17/2012 - FINAL PUD - PHASE 1
 03/18/2013 - REVISION

STAMP
 REGISTERED PROFESSIONAL
 ENGINEER
 56481
 Matthew M. Keenan
 OREGON
 EXPIRES 12/31/2015
 MATTHEW M. KEENAN

EXPIRATION DATE: 6/30/13
 PROJECT #: 307027.00
 DRAWN BY: AMF
 CHECKED BY: MK
 FILE NAME:
 DATE: 03/18/2013

CANNERY STATION

EC3.0
 EROSION CONTROL
 DETAILS



**DOUGHERTY
LANDSCAPE
ARCHITECTS**

474 Wilburton Street
Suite 305
Eugene, Oregon 97401

P 541.683.5803

F 541.683.8183

www.DLAdesign.com

PROJECT:

**CANNERY STATION
FLORENCE, OR**

**FINAL PUD - PHASE 1
LOTS 1 - 6**

CLIENT:



arlie@company

ARLIE & COMPANY
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ISSUED:

10/14/2008 - TENTATIVE PUD

6/18/2012 - FINAL PUD - PHASE 1

STAMP:



PROJECT #: 728

DRAWN BY:

CHECKED BY:

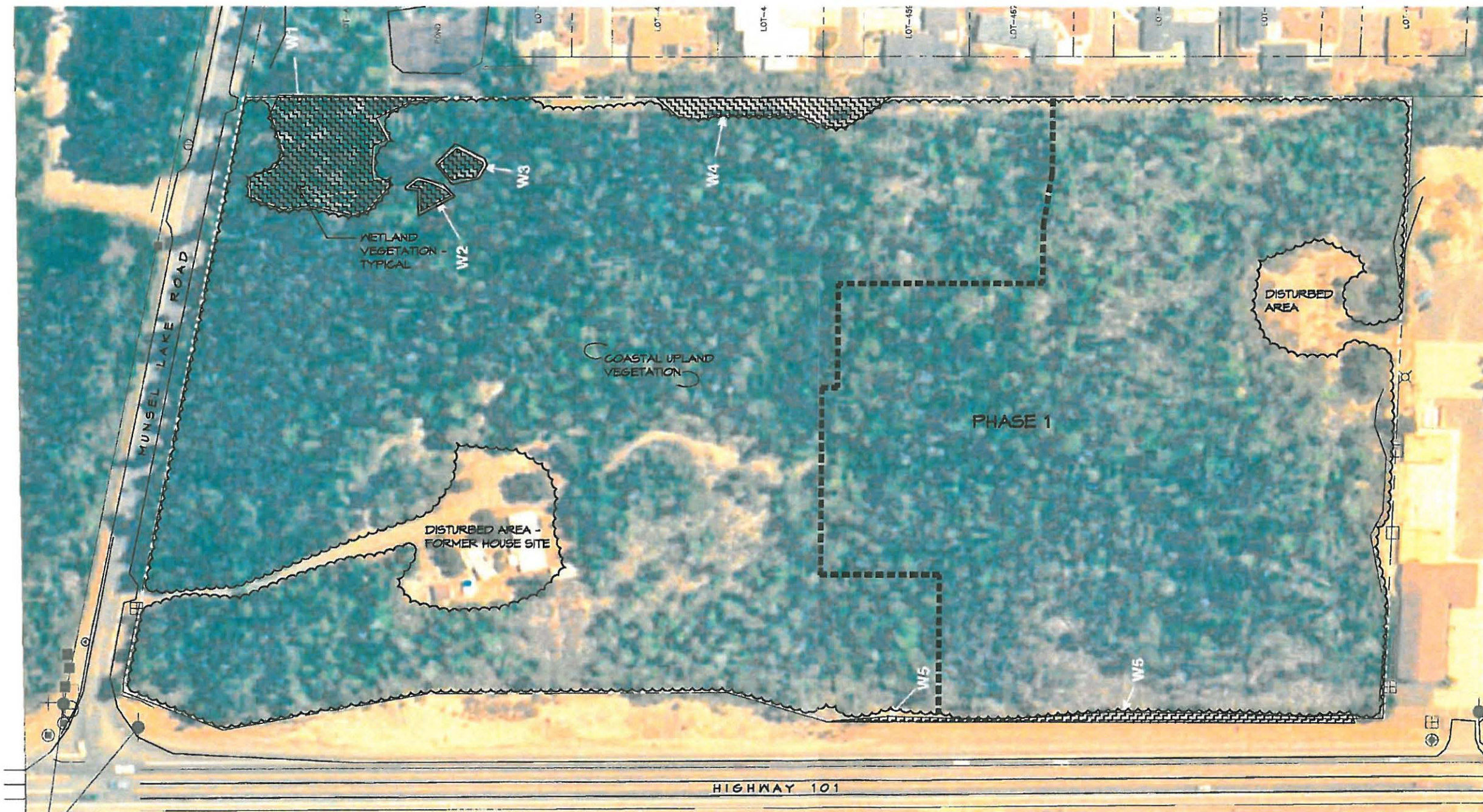
FILE NAME:

DATE: 9/18/2012

CANNERY STATION

DRAWING:

LA-1



LEGEND



COASTAL UPLAND VEGETATION - DOMINANT PLANT SPECIES LISTED BELOW

TREES

- Pinus contorta* (Shore Pine)
- Pseudotsuga menziesii* (Douglas Fir)

UNDERSTORY

- Arctostaphylos columbiana* (Hairy Manzanita)
- Gaultheria shallon* (Salal)
- Myrica californica* (Pacific Wax Myrtle)
- Polystichum munitum* (Sword Fern)
- Rhododendron macrophyllum* (Coastal Rhododendron)
- Vaccinium ovatum* (Evergreen Huckleberry)



WETLAND VEGETATION - DOMINANT PLANT SPECIES LISTED BELOW (DATA IS BASED ON WETLAND DELINEATION REPORT, OCTOBER 2007 BY ENVIRONMENTAL SOLUTIONS, LLC.)

TREES

- Alnus rubra* (Red Alder)
- Malus fusca* (Pacific Crab Apple)
- Pinus contorta* (Shore Pine)
- Salix* species (Willow)

UNDERSTORY

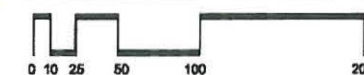
- Carex obnupta* (Slough Sedge)
- Gaultheria shallon* (Salal)
- Myrica californica* (Pacific Wax Myrtle)
- Rhododendron neoglandulosum* (Smooth Labrador-tea)
- Rosa nutkana* (Nutmeg Rose)
- Spiraea douglasii* (Douglas Spiraea)

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City of Florence
Community Development
Department
33
Exhibit
PC1212FPu001
File Number

NOTES:

1. COASTAL RHODODENDRONS ARE FOUND THROUGHOUT THE HIGHER AREAS OF THE SITE WITH DENSITIES RANGING FROM ZERO TO TEN PLANTS PER 1000 SQ. FEET. LOWER AREAS AND WETLANDS ARE MOSTLY DEVOID OF RHODODENDRONS.
2. LIMITS OF VEGETATION TO REMAIN ARE PENDING DEVELOPMENT OF ENGINEER'S GRADING PLAN.

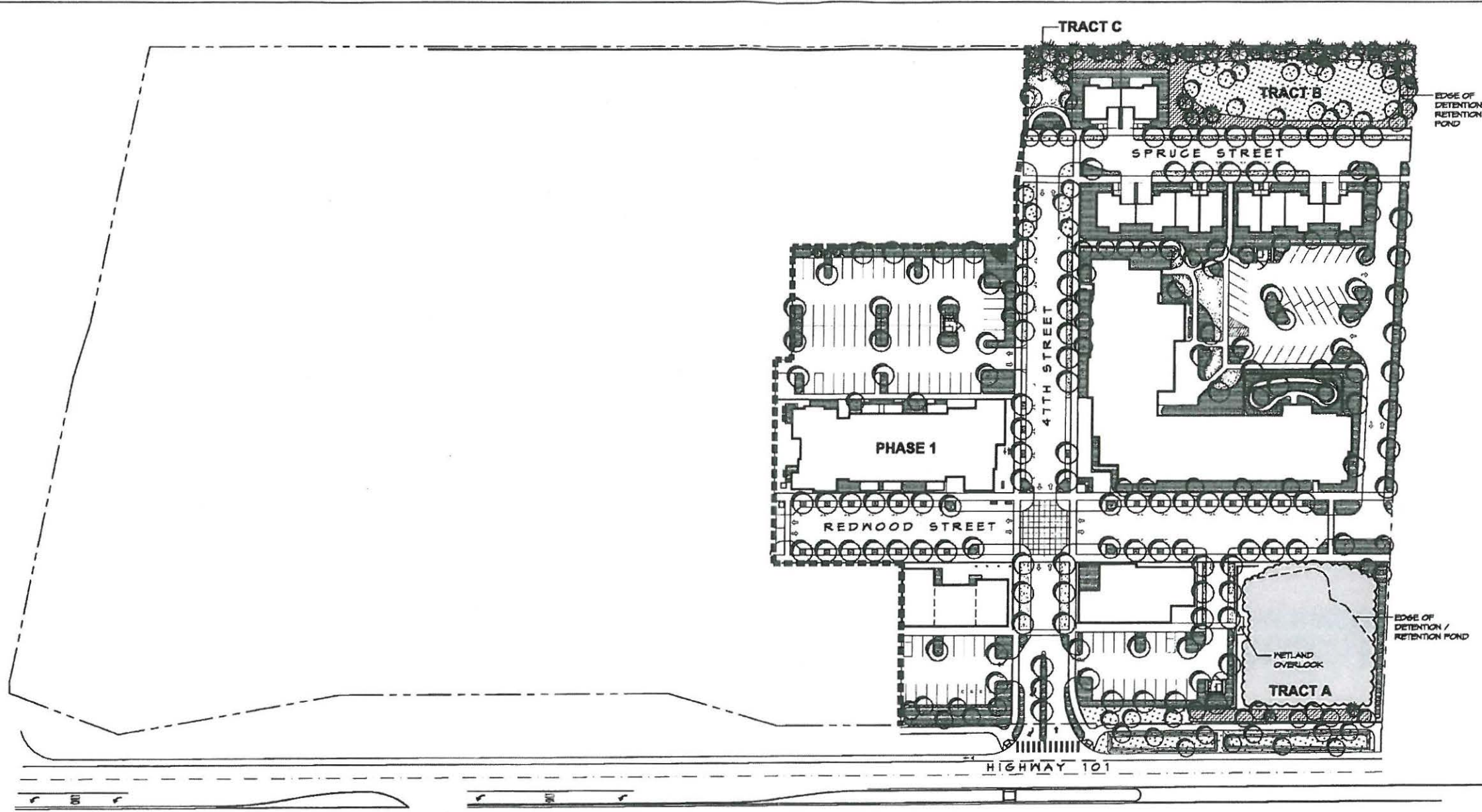
EXISTING VEGETATION PLAN



SCALE: 1" = 50'-0"



NORTH



LEGEND

- | | | | |
|--|---|--|--|
| | PROPOSED DECIDUOUS TREES | | NATIVE PLANTING AREAS |
| | PROPOSED CONIFERS | | LAWN AREAS |
| | GENERAL PLANTING AREAS | | EXISTING VEGETATION TO REMAIN TO EXTENT POSSIBLE - SEE SHEET LA-1 FOR DOMINANT PLANT SPECIES |
| | STORMWATER TREATMENT AND LOW AREA PLANTINGS | | |

NOTES:

- SEE SHEET LA-3 FOR PLANT LISTS THAT CORRESPOND TO PLANTING AREA SYMBOLS.
- HIGH EFFICIENCY IRRIGATION WILL BE PROVIDED AS REQUIRED TO ENSURE PLANT ESTABLISHMENT AND ONGOING PLANT HEALTH. NEITHER IRRIGATION NOR ASSOCIATED TRENCHING IS TO OCCUR IN AREAS OF EXISTING VEGETATION PRESERVATION.

LANDSCAPE MASTER PLAN



APPROVED
City of Florence
Community Development
Department
 34
 Exhibit
 PC1212 FPU D01
 File Number

Rowell Brokaw Architects

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 One East Broadway, Suite 110
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 Phone (541) 485-1883
 Fax (541) 485-7314
 www.rowellbrokaw.com



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PROJECT:
CANNERY STATION
FLORENCE, OR

FINAL PUD - PHASE 1
 LOTS 1 - 6

CLIENT:



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REVISIONS:
 10/14/2008 - TENTATIVE PUD
 07/18/2012 - FINAL PUD - PHASE 1



PROJECT # 123
 DRAWN BY:
 CHECKED BY:
 FILE NAME:
 DATE: 07/18/2012

CANNERY STATION

DRAWING:

LA-2



GENERAL PLANT LIST

SPACING	BOTANICAL NAME	COMMON NAME	MIN. SIZE	COMMENTS
---------	----------------	-------------	-----------	----------

DECIDUOUS TREES

Per Plan	Acer circinatum	Vine Maple	6' ht.	multi-stem, B&B
Per Plan	Acer rubrum 'Red Sunset'	Red Sunset Maple	2' cal.	matching, full
Per Plan	Carpinus betula 'Fastigiata'	Columar Hornbeam	2' cal.	matching, full
Per Plan	Fraxinus americana 'Linginger'	Autumn Purple Ash	2' cal.	matching, full
Per Plan	Fraxinus oxycarpa 'Raywood'	Raywood Ash	2' cal.	matching, full
Per Plan	Nyssa sylvatica	Black Tupelo	2' cal.	matching, full
Per Plan	Prunus serrulata 'Kwanzan'	Kwanzan Cherry	1.75' cal.	single stem, full
Per Plan	Pyrus calleryana 'Aristocrat'	Ornamental Pear	1.75' cal.	matching, full
Per Plan	Tilia cordata 'Greenspire'	Greenspire Linden	2' cal.	matching, full
Per Plan	Zelkova serrata 'Green Vase'	Green Vase Zelkova	2' cal.	matching, full

CONIFER TREES

Per Plan	Pinus contorta 'Contorta'	Shore Pine	6' ht.	matching, full
Per Plan	Pseudotsuga menziesii	Douglas Fir	6' ht.	matching, full
Per Plan	Sequoia sempervirens	Coast Redwood	6' ht.	matching, full
Per Plan	Thuja plicata	Western Red Cedar	6' ht.	matching, full

SHRUBS

5' o.c.	Abelia grandiflora 'Edward Goucher'	Edward Goucher Glossy Abelia	5 gal.	matching, full
30' o.c.	Berberis thunbergii 'Crimson Pigeon'	Dwarf Japanese Barberry	5 gal.	matching, full
5' o.c.	Ceanothus thyrsiflorus	Blue Blossom Ceanothus	5 gal.	matching, full
5' o.c.	Cornus sericea	Redtwig Dogwood	5 gal.	matching, full
5' o.c.	Cornus sericea 'Flaviramea'	Yellowtwig Dogwood	30" ht.	matching, full
5' o.c.	Cornus sericea 'Kelsey'	Dwarf Redtwig Dogwood	5 gal.	matching, full
30' o.c.	Calluna vulgaris 'Aurea'	Aurea Scotch Heather	5 gal.	matching, full
5' o.c.	Daphne odora	Winter Daphne	5 gal.	matching, full
4' o.c.	Escallonia compacta	Compact Escallonia	5 gal.	matching, full
4' o.c.	Eurythmus alata 'Compacta'	Burning Bush	5 gal.	matching, full
5' o.c.	Escallonia Newport Dwarf	Newport Dwarf Escallonia	5 gal.	matching, full
4' o.c.	Fuchsia magellanica	Hardy Fuchsia	5 gal.	matching, full
5' o.c.	Gaultheria shallon	Salei	5 gal.	matching, full
4' o.c.	Hydrangea macrophylla	Big-leaf Hydrangea	5 gal.	matching, full
4' o.c.	Hydrangea quercifolia	Oakleaf Hydrangea	5 gal.	matching, full
5' o.c.	Hebe Autumn Glory	Autumn Glory Hebe	5 gal.	matching, full
5' o.c.	Hebe 'Pat's Purple'	Pat's Purple Hebe	5 gal.	matching, full
6' o.c.	Hibiscus syriacus	Rose of Sharon	7 gal.	matching, full
7' o.c.	Myrica californica	Pacific Wax Myrtle	7 gal.	matching, full
4' o.c.	Prunus laurocerasus 'Otto Luyken'	Otto Luyken English Laurel	5 gal.	matching, full
4' o.c.	Pittosporum tobira	Tobira Pittosporum	5 gal.	matching, full
5' o.c.	Rhododendron 'Anah Kruische'	Anah Kruische Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Blue Diamond'	Blue Diamond Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Christmas Cheer'	Christmas Cheer Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Dora Amata's'	Dora Amata's Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Gomer Waterer'	Gomer Waterer Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Hallelujah'	Hallelujah Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron 'Loder's White'	Loder's White Rhododendron	5 gal.	matching, full
5' o.c.	Rhododendron macrophyllum	Coast Rhododendron	6' ht.	matching, full
4' o.c.	Rhododendron 'PJM'	PJM Rhododendron	5 gal.	matching, full
4' o.c.	Rhododendron 'Unique'	Unique Rhododendron	5 gal.	matching, full
4' o.c.	Rosa rugosa	Sea Tomato	24" ht.	matching, full
4' o.c.	Vaccinium ovatum	Evergreen Huckleberry	5 gal.	matching, full
5' o.c.	Viburnum davidii	David Viburnum	5 gal.	matching, full
4' o.c.	Viburnum tinus 'Spring Bouquet'	Spring Bouquet Viburnum	5 gal.	matching, full

GROUNDCOVERS AND LOW PLANTINGS

24" o.c.	Arctostaphylos uva-ursi	Kinnikinnick	1 gal.	matching, full
24" o.c.	Bergenia crassifolia	Heartleaf Bergenia	1 gal.	matching, full
30" o.c.	Calamagrostis acutiflora 'stricta'	Feather Reed Grass	1 gal.	matching, full
24" o.c.	Ceanothus glaucus	-	2 gal.	matching, full
30" o.c.	Erica carnea 'Springwood Pink'	Pink Springwood Heather	1 gal.	matching, full
30" o.c.	Erica darleyensis 'Turzey'	Mediterranean White Heather	1 gal.	matching, full
30" o.c.	Calamagrostis acutiflora 'Karl Forester'	Feather Reed Grass	1 gal.	matching, full
24" o.c.	Festuca glauca	Blue Fescue	1 gal.	matching, full
18" o.c.	Fragaria chiloensis	Beach Strawberry	1 gal.	matching, full
24" o.c.	Helleborus orientalis	Lenten Rose	1 gal.	matching, full
24" o.c.	Helictotrichon sempervirens	Blue Oat Grass	1 gal.	matching, full
24" o.c.	Hemerocallis 'Stella D' Oro'	Stella D' Oro Daylily	1 gal.	matching, full
18" o.c.	Imperata cylindrica 'Rubra'	Japanese Blood Grass	1 gal.	matching, full
30" o.c.	Levander angustifolia	English Lavender	1 gal.	matching, full
24" o.c.	Levander angustifolia 'Hurstead'	Dwarf English Lavender	1 gal.	matching, full
18" o.c.	Liriope muscari	Big Blue Lily Turf	1 gal.	matching, full
30" o.c.	Lonicera pileata	Privet Honeyuckle	5 gal.	matching, full
30" o.c.	Miscanthus sinensis 'Gracillimus'	Maiden Grass	1 gal.	matching, full
30" o.c.	Pennisetum alopecuroides	Fountain Grass	1 gal.	matching, full
18" o.c.	Rudbeckia hirta	Black-eyed Susan	1 gal.	matching, full
18" o.c.	Rubus calycinoides 'Emerald Carpet'	Rubus	1 gal.	matching, full

STORMWATER TREATMENT PLANT LIST

(THESE PLANTS TO BE USED IN BIO-SWALES AND OTHER DESIGNATED STORMWATER TREATMENT AREAS)

SPACING	BOTANICAL NAME	COMMON NAME	MIN. SIZE	COMMENTS
---------	----------------	-------------	-----------	----------

DECIDUOUS TREES

Per Plan	Acer macrophyllum	Big leaf Maple	2' cal.	matching, full
Per Plan	Acer rubrum	Red Maple	2' cal.	matching, full
Per Plan	Alnus rubra	Red Alder	1.75' cal.	matching, full
Per Plan	Cornus douglasii	Black Hawthorn (Midland Form)	7' ht.	matching, full
Per Plan	Fraxinus latifolia	Oregon Ash	1.75' cal.	matching, full
Per Plan	Nyssa sylvatica	Black Tupelo	2' cal.	matching, full

SHRUBS

4'-6' o.c.	Cornus sericea	Redtwig Dogwood	5 gal.	matching, full
5'-6' o.c.	Cornus sericea 'sant'	Sant' Dogwood	5 gal.	matching, full
5'-6' o.c.	Mahonia aquifolium	Oregon Grape	5 gal.	matching, full
5' o.c.	Myrica californica	Pacific Wax Myrtle	7 gal.	matching, full
4' o.c.	Rosa nutkana	Nootka Rose	5 gal.	matching, full
4'-6' o.c.	Salix purpurea	Alaska Willow	5 gal.	matching, full
5'-6' o.c.	Symphoricarpos albus	Snowberry	5 gal.	matching, full
5' o.c.	Spirea douglasii	Douglas Spirea	5 gal.	matching, full

GROUNDCOVERS AND BULBS

Per Plan	Camaeola quamesh	Camae	bulb	large
30" o.c.	Carex densa	Dense Sedge	1 gal.	matching, full
30" o.c.	Carex obovata	Slough Sedge	1 gal.	matching, full
30" o.c.	Cornus sericea 'Kelsey'	'Kelsey' Dogwood	5 gal.	matching, full
30" o.c.	Juncus effusus	-	1 gal.	matching, full
30" o.c.	Juncus patens	-	1 gal.	matching, full

NATIVE PLANT LIST

(THESE PLANTS ARE ALL NATIVE TO THE OREGON COAST ZONE. THEY ARE TO BE USED FOR PERIMETER PLANTING BUFFERS AND NATURAL AREAS. THESE PLANTS MAY ALSO BE USED WITHIN PLANTING COMPOSITIONS THROUGHOUT THE SITE.)

SPACING	BOTANICAL NAME	COMMON NAME	MIN. SIZE	COMMENTS
---------	----------------	-------------	-----------	----------

DECIDUOUS TREES

Per Plan	Acer circinatum	Vine Maple	6' ht.	multi-stem, B&B
Per Plan	Acer macrophyllum	Big Leaf Maple	2' cal.	matching, full
Per Plan	Alnus rhombifolia	White Alder	2' cal.	matching, full
Per Plan	Alnus rubra	Red Alder	1.75' cal.	matching, full
Per Plan	Cornus nuttallii	Pacific Dogwood	1.75' cal.	matching, full
Per Plan	Fraxinus latifolia	Oregon Ash	1.5' cal.	matching, full
Per Plan	Rhamnus purshiana	Cascara Buckthorn	3 gal./30" br.	matching, full

CONIFER TREES

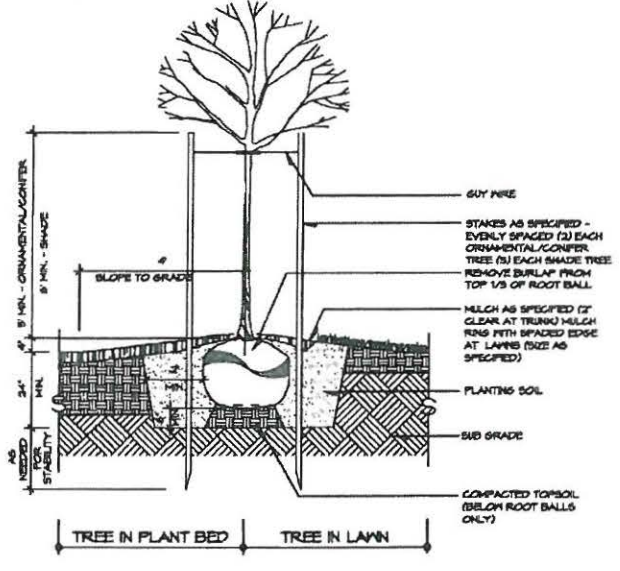
Per Plan	Picea sitchensis	Sitka Spruce	6' ht.	matching, full
Per Plan	Pinus contorta 'Contorta'	Shore Pine	6' ht.	matching, full
Per Plan	Pseudotsuga menziesii	Douglas Fir	6' ht.	matching, full
Per Plan	Thuja plicata	Western Red Cedar	6' ht.	matching, full
Per Plan	Taxus heterophylla	Western Hemlock	6' ht.	matching, full

SHRUBS

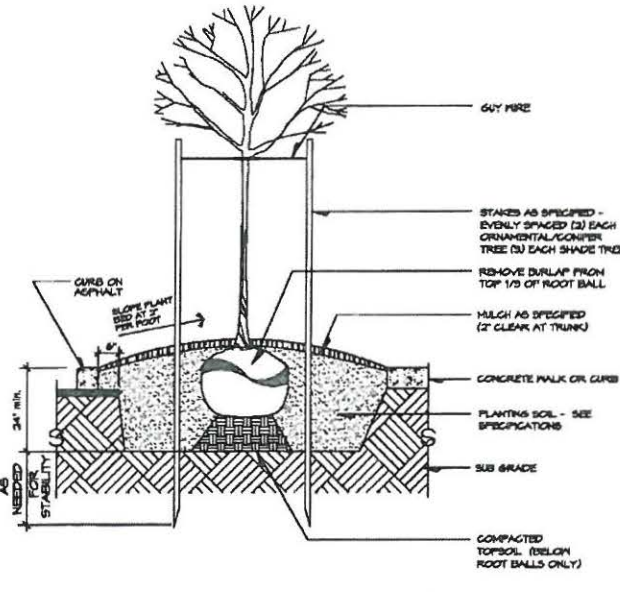
7' o.c.	Ametanchier sinifolia	Western Serviceberry	30" ht.	matching, full
4'-6' o.c.	Cornus sericea	Red-Osier Dogwood	30" ht.	matching, full
5' o.c.	Gaultheria shallon	Salei	1 gal.	matching, full
7' o.c.	Holodiscus discolor	Oceanapry	30" ht.	matching, full
5'-6' o.c.	Mahonia aquifolium	Oregon Grape	5 gal.	matching, full
Per Plan	Malus fusca	Pacific Crabapple	7 gal.	matching, full
7' o.c.	Myrica californica	Pacific Wax Myrtle	4' ht.	matching, full
7' o.c.	Oenothera cernuiformis	Indian Plum	4' ht.	matching, full
7' o.c.	Philadelphus lewisii	Native Mock Orange	4' ht.	matching, full
7' o.c.	Physocarpus capitatus	Pacific Nine Bark	4' ht.	matching, full
50' o.c.	Polytichum munium	Sword Fern	5 gal.	matching, full
6' o.c.	Rhododendron macrophyllum	Coast Rhododendron	5' ht.	matching, full
4' o.c.	Rhododendron neoglandulosum	Labrador Tea	30" ht.	matching, full
6' o.c.	Ribes sanguineum	Red Flowering Currant	5 gal.	matching, full
4' o.c.	Rosa nutkana	Nootka Rose	5' ht.	matching, full
5' o.c.	Salix hookeriana	Coast Willow	4' ht.	matching, full
5' o.c.	Spirea douglasii	Western spirea	5-4' ht.	matching, full
5'-6' o.c.	Symphoricarpos albus	Snowberry	5 gal.	matching, full
4' o.c.	Vaccinium ovatum	Evergreen Huckleberry	5 gal.	matching, full

GROUNDCOVERS

24" o.c.	Arctostaphylos uva-ursi	Kinnikinnick	1 gal.	matching, full
18" o.c.	Fragaria chiloensis	Beach Strawberry	1 gal.	matching, full
30" o.c.	Mahonia repens	Crawling Mahonia	1 gal.	matching, full



1 TYPICAL TREE PLANTING AT PLANT BEDS AND LAWNS
 SECTION SCALE: 1/2" = 1'-0"



2 TREE PLANTING AT PARKING AREAS
 SECTION SCALE: 1/2" = 1'-0"

GENERAL NOTE:
 THE PLANT MATERIAL FOR PROJECT LANDSCAPING TO BE SELECTED PRIMARILY FROM THESE LISTS. ADDITIONAL PLANT MATERIAL MAY BE INTRODUCED UPON FURTHER DESIGN DEVELOPMENT SUBJECT TO APPROVAL BY CITY STAFF.

APPROVED
 City of Florence
 Community Development
 Department
 35 Exhibit
 REIZIZ FPU001
 File Number

PLANT LISTS AND DETAILS



**DOUGHERTY
LANDSCAPE
ARCHITECTS**

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P 541.683.5803
F 541.683.8183

www.DLAdesign.com

PROJECT:
**CANNERY STATION
FLORENCE, OR**

CONSTRUCTION REVISIONS

CLIENT:



ARLE & COMPANY
2811 TENNYSON AVENUE
SUITE 400
EUGENE, OREGON 97408
(541) 344-6500
www.arle.com

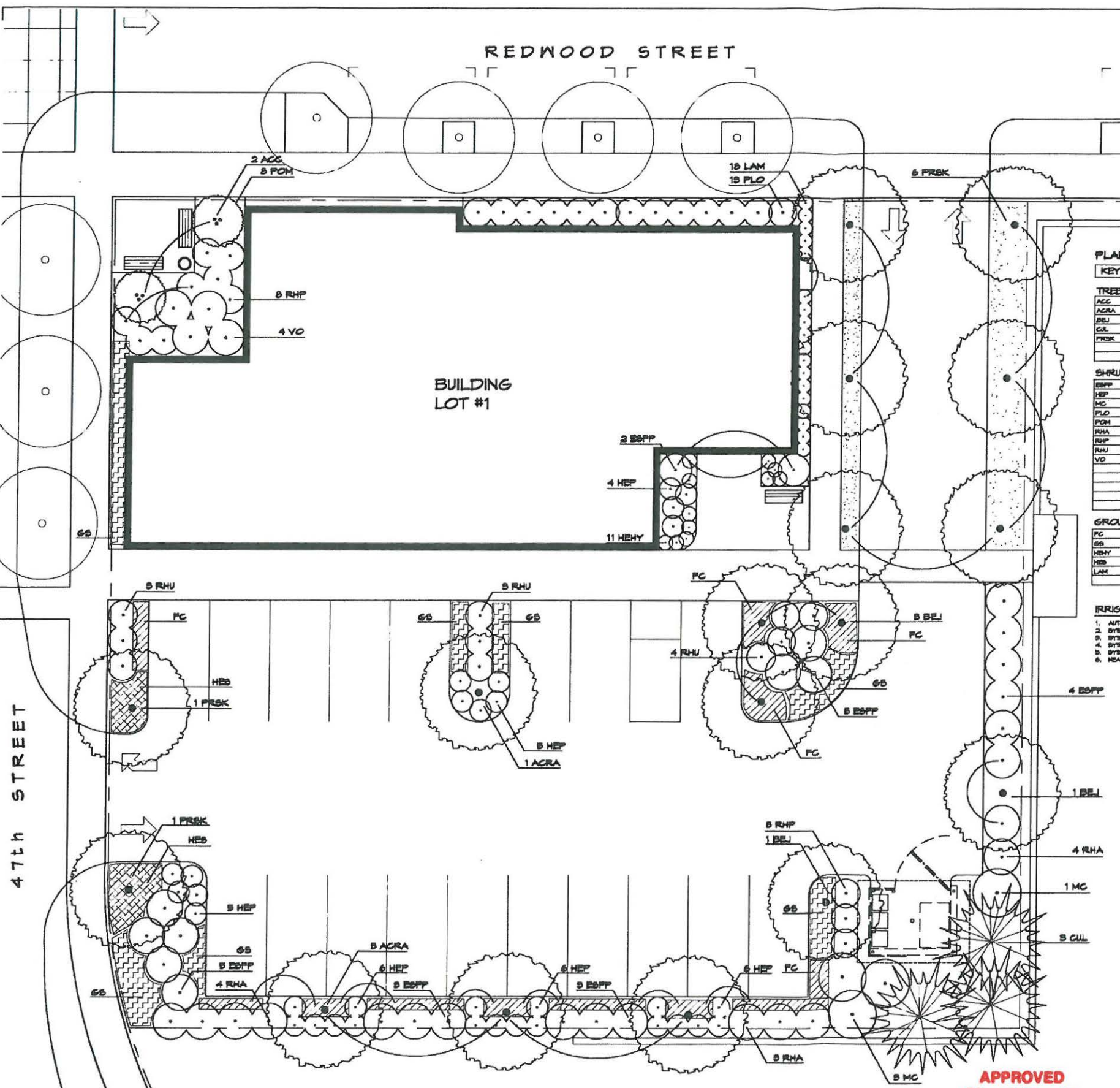
ISSUED:



PROJECT #: 123
DRAWN BY:
CHECKED BY:
FILE NAME:
DATE: 3/12/13
CANNERY STATION
DRAWING:

L-1

Exhibit 36



PLANT SCHEDULE

KEY	QTY.	BOTANICAL NAME	COMMON NAME	MIN. SIZE	COMMENTS
TREES					
ACC	2	Acer chinensis	Yucca Maple	4' ht.	multi-stem, full
ACRA	4	Acer rubrum 'Autumn Flame'	Autumn Flame Maple	2' cal.	Matching, spaced up to approx. 6 ft.
BEJ	3	Bobula jacquemontii	Jacquemont's Birch	1.5' cal.	Matching, spaced up to approx. 6 ft.
CUL	5	X Cupressocypripis 'Legend'	Legend Cypress	4' ht.	Matching, full, B&B
FRSK	6	Prunus serotina 'Kauzner'	Kauzner Cherry	2' cal.	single stem, B&B, matching, full
SHRUBS					
ESPP	22	Eucalyptus 'Princess' 'Pink Princess'	Pink Princess Eucalyptus	5 gal.	matching, full
HEP	53	Hebe 'Tidys Purple'	Purple Hebe	5 gal.	matching, full
MC	4	Myrica californica	Pacific Wax Myrtle	5 gal.	matching, full
FLO	15	Prunus laurocerasus 'Ocho Lujon'	Ocho Lujon English Laurel	5 gal.	matching, full
POM	5	Polygonatum multiflorum	Sword Fern	5 gal.	matching, full
RHA	11	Rhododendron 'Anah Kruische'	Anah Kruische Rhododendron	5 gal.	matching, full
RHP	11	Rhododendron 'PJM'	PJM Rhododendron	5 gal.	matching, full
RUH	10	Rhododendron 'Unique'	Unique Rhododendron	5 gal.	matching, full
VO	4	Vaccinium ovatum	Evergreen Huckleberry	5 gal.	matching, full
GROUNDCOVERS AND PERENNIALS					
FC	15' o.c.	Fragaria chiloensis	Beach Strawberry	4" pots	matching, full
GS	36" o.c.	Deschampsia aethiops	Orchard Grass	5 gal.	matching, full
HENY	11	Hemerocallis 'Hyperion'	Hyperion Daylily	1 gal.	matching, full
HES	15' o.c.	Hellandrolon sempervivans	Blue Oat Grass	1 gal.	matching, full
LAM	15	Lavandula angustifolia 'Munstead'	Dwarf English Lavender	1 gal.	matching, full

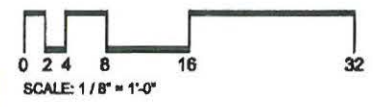
IRRIGATION SYSTEM DESCRIPTION

1. AUTOMATIC IRRIGATION SYSTEM WILL BE PROVIDED FOR ALL LAWN AND PLANT BED AREAS.
2. SYSTEM WILL BE AN UNDERGROUND IRRIGATION SYSTEM.
3. SYSTEM WILL PROVIDE HEAD TO HEAD COVERAGE.
4. SYSTEM WILL PROVIDE SEPARATE ZONES FOR LAWN AND SHRUB AREAS.
5. SYSTEM WILL PROVIDE SEPARATE ZONES FOR DIFFERENT SOLAR ORIENTATIONS.
6. HEAD LAYOUT WILL MINIMIZE OVERLAY ONTO PAVED SURFACES.

LEGEND

- PROPOSED DECIDUOUS TREES
- PROPOSED CONIFERS
- SALAL (GS)
- BEACH STRAWBERRY (FC)
- BENCH
- APPROX. STREET TREE LOCATION
- SHRUB PLANTING
- LAWN AREAS
- BLUE OAT GRASS (HES)
- FERNS
- LITTER RECEPTACLE

FOR DESIGN REVIEW 3-13-13
LANDSCAPE PLAN



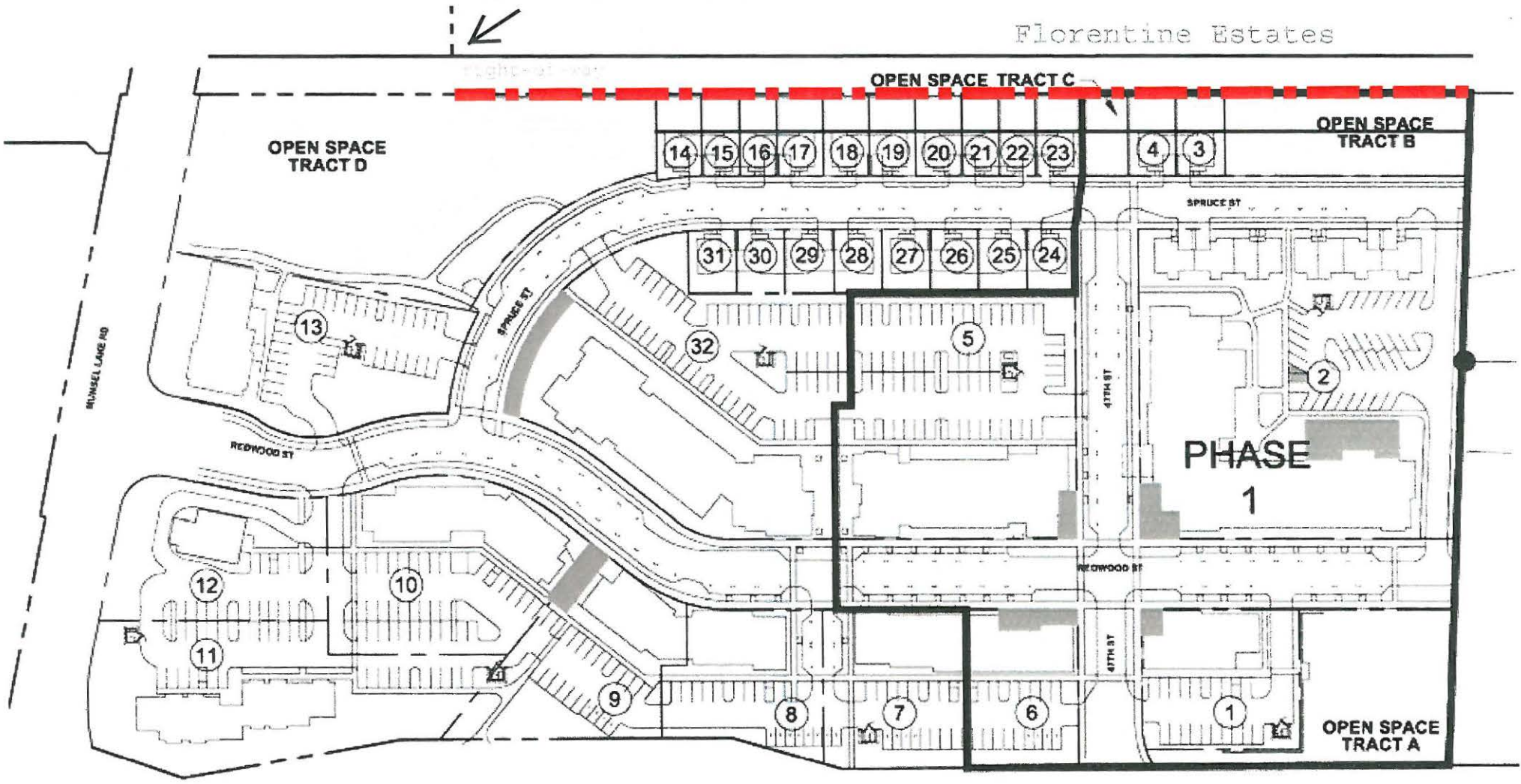
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City of Florence
Community Development
Department
36
Exhibit

PC1212.FPUD01
File Number

Exhibit 53: Extent of Wall

Northwest corner of lot 18-12-14-24 9300

Florentine Estates



--- Extent of Wall to be constructed as per Condition 2

HAY 105

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Exhibit
PC1212FPUD 01
File Number